

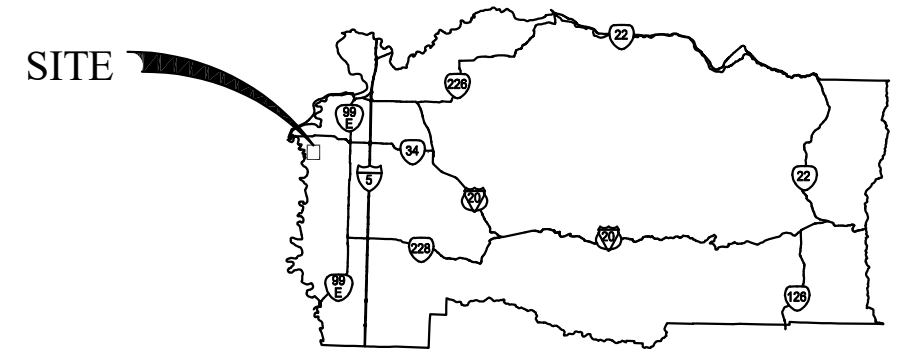
LINN COUNTY ROAD DEPARTMENT

OVERALL PROJECT LENGTH: 0.08 MILES

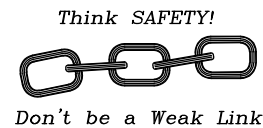
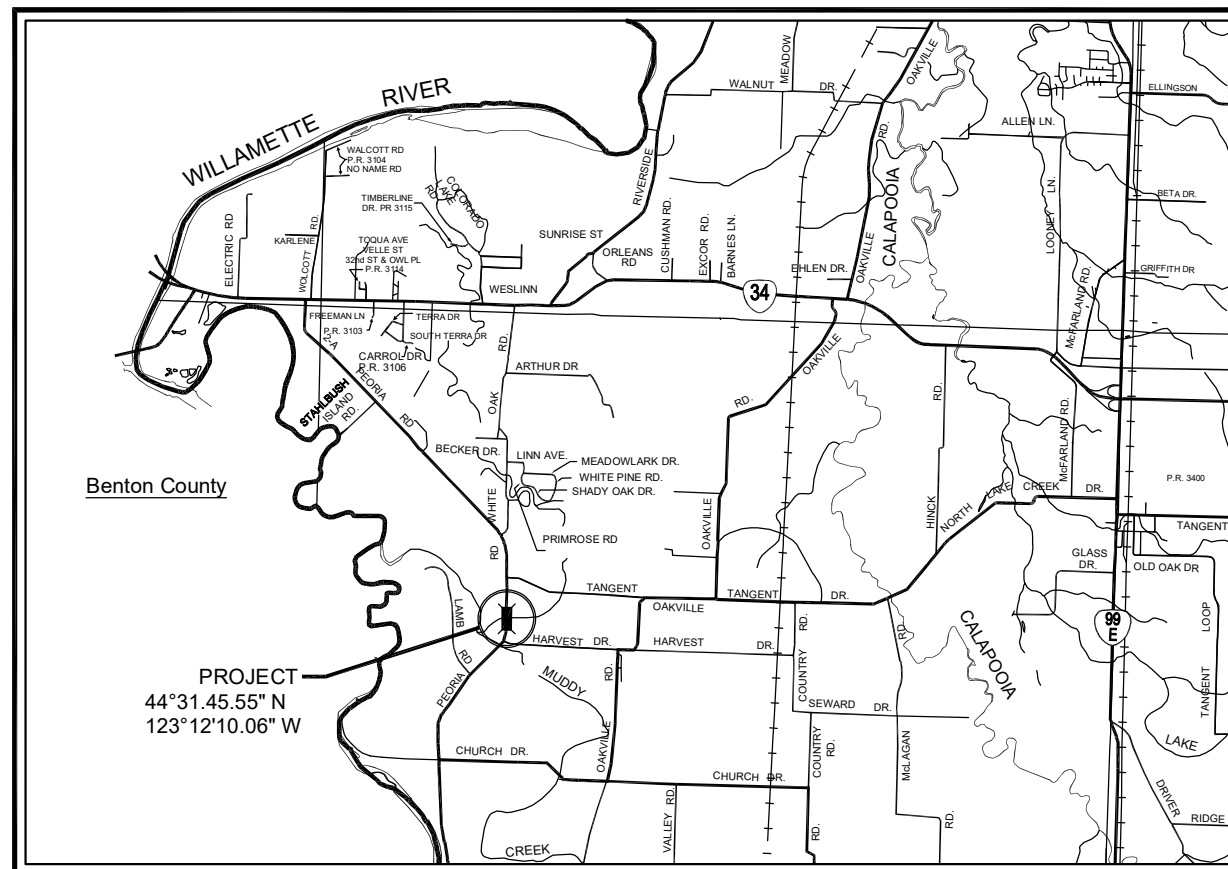
BRIDGES AND ROADWAYS OWL CREEK - PEORIA ROAD BRIDGE REPLACEMENT LINN COUNTY

LINN COUNTY BRIDGE NO. 0002-0306
ODOT BRIDGE NO. 24336

FEBRUARY 2025



PROJECT LOCATION



English Units

ATTENTION:
Oregon law requires you to follow rules adopted by the Oregon Utility Notification Center. Those rules are set forth in OAR 952-001-0010 through OAR 952-001-0090. You may obtain a copies of the rules by calling the center. (Note: the telephone number for the Oregon Utility Notification Center is (503) 232-1987.)



LINN COUNTY ROAD DEPARTMENT 3010 FERRY STREET SW ALBANY, OREGON 97322 PHONE: (541) 967-3919 FAX: (541) 924-0202 E-MAIL: Roads@co.linn.or.us	COUNTY COMMISSION ROGER NYQUIST CHAIRMAN WILLIAM TUCKER SHERRIE SPRENGER	ROADMASTER WAYNE E. MINK, P.E. COUNTY ENGINEER DAINEAL MALONE, P.E.
---	---	--

DATE:	REVISION:	BY:	BRIDGE NO:	DATE:
			0002-0306	1/13/2025
			PROJECT NO: CB2102	
			TRS: T. 12 S., R. 04 W., SECTION 17, W.M.	
			DESIGNED BY: K. Groom	CHECKED BY: A. Potts
			DRAFTED BY: K. Groom	REVIEWED BY: D. Malone

OWL CREEK - PEORIA ROAD BRIDGE REPLACEMENT LINN COUNTY 2025	TITLE SHEET SCALE: None SHEET 01
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1/13/2025 11:15 AM

SHEET INDEX	
SHEET NO.	TITLE
01	TITLE SHEET
02	SHEET INDEX, LEGEND, NOTES, STANDARD DRAWINGS & ABBREVIATIONS
RD-01	TRAFFIC CONTROL PLAN
RD-02	TYPICAL SECTIONS AND SUPERELEVATION DIAGRAM
RD-03	PLAN AND PROFILE
RD-04	EROSION CONTROL PLAN
RD-05	SIGNING AND STRIPING PLAN
RD-06	STORMWATER DRAINAGE PLAN
BR-01	PLAN AND ELEVATION
BR-02	GENERAL NOTES
BR-03	FOUNDATION DATA SHEET
BR-04	FOUNDATION PLAN AND DETAILS
BR-05	TYPICAL SECTION AND DECK PLAN
BR-06	SLAB DETAILS
BR-07	BENT 1 PLAN AND ELEVATION
BR-08	BENT DETAILS
BR-09	WINGWALL DETAILS

ABBREVIATION LEGEND			
AASHTO	AMERICAN ASSOCIATION OF STATE HIGHWAY TRANSPORTATION OFFICIALS	LT	LEFT
AC	ASPHALT CONCRETE	LRFD	LOAD AND RESISTANCE FACTOR DESIGN
ACP	ASPHALT CONCRETE PAVEMENT	LS	LUMP SUM
A.D.	ALGEBRAIC DIFFERENCE	M.I.	MALLEABLE IRON
ASTM	AMERICAN SOCIETY OF TESTING MATERIALS	MIN	MINIMUM
BP	BEGINNING OF PROJECT	MUTCD	MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES
BRG	BEARING	NOM	NOMINAL
BT	BENT	OD	OUTSIDE DIAMETER
BH	BORE HOLE	ODOT	OREGON DEPARTMENT OF TRANSPORTATION
BTM	BOTTOM	O.F.	OUTSIDE FACE
BVCE	BEGIN VERTICAL CURVE ELEVATION	OHW	ORDINARY HIGH WATER
BVCS	BEGIN VERTICAL CURVE STATION	PC	POINT OF CURVE
CJP	COMPLETE JOINT PENETRATION	PI	POINT OF INTERSECTION
C.G.	CENTER OF GRAVITY	PP	PIPE PILE
CL	CENTERLINE	P/S	PRESTRESS/PRESTRESSED
CTR	CENTER	PT	POINT OF TANGENT
DIA	DIAMETER	PVI	POINT OF VERTICAL INTERSECTION
EA	EACH	QPL	QUALIFIED PRODUCTS LIST
ELEV/EL	ELEVATION	R	RADIUS
EOP	EDGE OF PAVEMENT	RT	RIGHT
EP	END OF PROJECT	SHLD	SHOULDER
EVCE	END VERTICAL CURVE ELEVATION	SHT	SHEET
EVCX	END VERTICAL CURVE STATION	SL	SLOPE
EXTG/EX	EXISTING	STA	STATION
FHWA	FEDERAL HIGHWAY ADMINISTRATION	STD	STANDARD
IE	INVERT ELEVATION	T	TANGENT DISTANCE
I.F.	INSIDE FACE	TYP	TYPICAL
K	LENGTH OF CURVE/DIFFERENCE IN GRADE	VC	VERTICAL CURVE
		VERT	VERTICAL

LEGEND	
	500 CONTOUR
	ROW RIGHT OF WAY
	EXISTING TAX LOT LINE
	CENTERLINE
	EXISTING EDGE OF PAVEMENT
	EXISTING SHOULDER
	EXISTING DRIVEWAY
	EXISTING OVERHEAD POWER LINE
	EXISTING TELEPHONE LINE
	EXISTING GAS LINE
	EXISTING FENCE
	EXISTING GUARDRAIL
	EXISTING TOP OF BANK
	EXISTING STREAM
	CONSTRUCTION CENTERLINE
	EDGE OF PAVEMENT
	SHOULDER
	CURB
	GUARDRAIL
	DITCH
	WETLAND BOUNDARY
	NO WORK ZONE FENCING
	SAWCUT LINE
	TOE OF CONSTRUCTION FILL
	SUBGRADE GEOTEXTILE
	EXISTING VEGETATION TO BE REMOVED
	EXISTING VEGETATION TO REMAIN
	EXISTING SURVEY MONUMENT
	EXISTING SURVEY CONTROL
	EXISTING SIGN AND POST
	EXISTING UTILITY POLE AND GUY
	EXISTING TELEPHONE PEDESTAL
	EXISTING STRUCTURE
	CONCRETE
	ASPHALT CONCRETE PAVEMENT (ACP)

NOTES:

- TAXLOT LINES AND INFORMATION, AND EXISTING STRUCTURES ARE SHOWN FOR REFERENCE PURPOSES ONLY.
- PROTECT ALL SURVEY MONUMENTS AND PROPERTY PINS.
- UNLESS OTHERWISE NOTED OR ORDERED BY THE ENGINEER, CLEAR AND GRUB TO THE TOE OF THE CONSTRUCTION FILL.
- UNLESS OTHERWISE NOTED OR ORDERED BY THE ENGINEER: CUT, DECK, AND PLACE ALL TREES 10 INCHES OR LARGER IN DIAMETER AT A LOCATION SELECTED BY THE RESPECTIVE PROPERTY OWNER. THE TREE SYMBOLS SHOWN DO NOT REPRESENT ACTUAL TREES OR QUANTITIES. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO VISIT THE SITE PRIOR TO PLACING A BID IN ORDER TO DETERMINE THE EXACT SIZE AND QUANTITIES OF THE TREES AND OTHER MATERIALS THAT WILL NEED TO BE CLEARED AND GRUBBED.

ODOT STANDARD DRAWING NUMBERS:

BRIDGE

- BR165 Bridge Approach Slab
- BR200 Type "F" Concrete Rail
- BR203 Transition Type "F" Concrete Rail to Guardrail
- BR420 26" Precast Prestressed Slab
- BR445 Precast Prestressed Boxes and Slabs Details

MISCELLANEOUS

- RD100 Mailbox Support

GUARDRAIL

- RD402 Midwest Guardrail System Types
- RD404 Midwest Guardrail System Steel Post and Block
- RD406 Placement of Guardrails on Slopes
- RD409 Thrie Beam Guardrail
- RD410 Thrie Beam Guardrail Transition
- RD416 Midwest Guardrail System Standard Hardware (Nuts, Bolts, Washers and Misc.)
- RD417 Midwest Guardrail System End Sections
- RD420 Midwest Guardrail System Non-Flared Energy-Absorbing Terminal
- RD442 Midwest Guardrail System Typical Layout at Bridge Ends
- RD481 Midwest Guardrail System Height Conversion

EROSION CONTROL

- RD1030 Sediment Barrier Type 2, 3 and 4

PERMANENT SIGNING

- TM200 Sign Installation Details
- TM201 Miscellaneous Sign Placement Details

PAVEMENT MARKINGS

- TM500 Pavement Marking Standard Detail Blocks

SIGN, ILLUMINATION AND SIGNAL SUPPORT STRUCTURES

- TM676 Sign Attachments
- TM681 Perforated Steel Square Tube (PSST) Sign Support Installation
- TM687 Perforated Steel Square Tube (PSST) Anchor Foundation

TEMPORARY TRAFFIC CONTROL

- TM821 Temporary Sign Supports
- TM822 Temporary Sign Supports

ODOT STANDARD DETAIL NUMBERS:

EMBANKMENTS/CUTS

- DET2100 Standard Embankment Construction
- DET2101 Sliver Fill Benching Details

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			DESIGNED BY:	K. Groom	CHECKED BY:	A. Potts
			DRAFTED BY:	K. Groom	REVIEWED BY:	D. Malone

OWL CREEK - PEORIA ROAD
 BRIDGE REPLACEMENT
 LINN COUNTY
 2025

SHEET INDEX, LEGEND, NOTES,
 STANDARD DRAWINGS &
 ABBREVIATIONS
 SCALE: None SHEET 02



Notes:

Place water filled Jersey type barricades partial width of road. Barricades to be located at approximate limits of work.

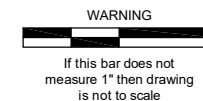
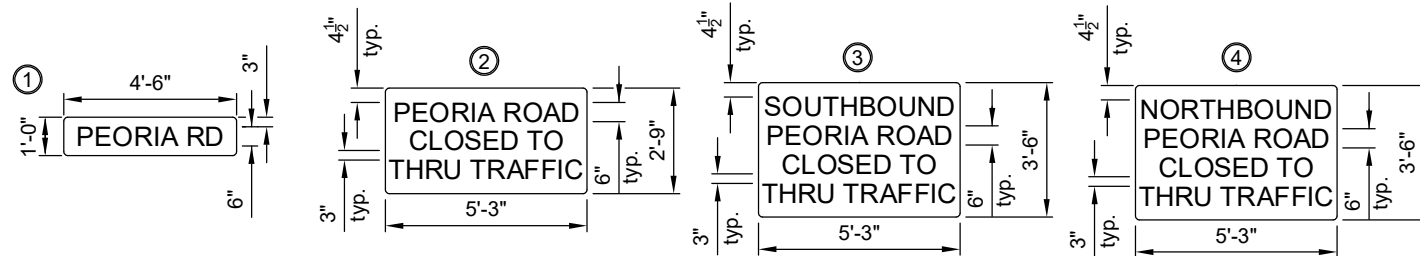
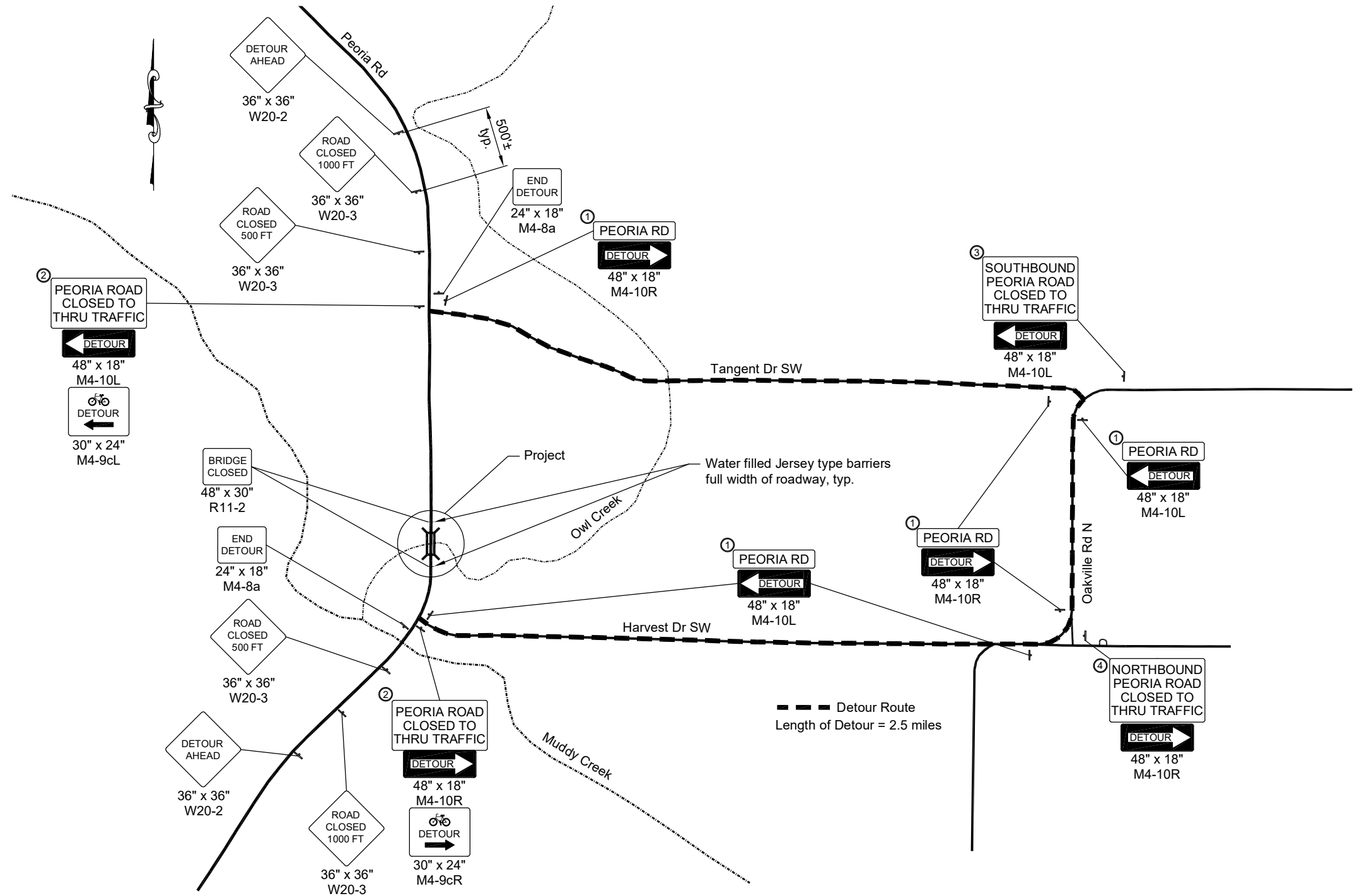
Water filled Jersey type barricade requirements:

- Barricades to extend to the full width of the road minus a 10' opening for residential traffic to use
- Barricades to include flashing warning light
- "BRIDGE OUT" signs 48" x 30" to be placed on front of barricades on each end of the road closure
- Barricades will be water filled (YODOCK Type III Kit in conjunction with Model 2001 or 2001 MB barricade or similar)
- Barricades and signs will meet the current edition of the MUTCD

Retain reasonable and safe access to all residential driveways within the project limits throughout the entire duration of the project.

Install temporary signs per ODOT Standard Drawings TM821 and TM822.

Install Portable Changeable Message Signs (PCMS) approximately 0.25 miles to the north and south of the project at least 14 days prior to the road closure. Refer to 0022.45(b) of the Special Provisions for requirements.



REGISTERED PROFESSIONAL ENGINEER
17206PE
Digitally signed by Kevin M. Groom
Date: 2025.01.13 11:30:47-08'00'
OREGON
JULY 19, 1994
KEVIN M. GROOM
Expires: 6/30/25



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DAINEAL MALONE, P.E.

DATE:	REVISION:	BY:

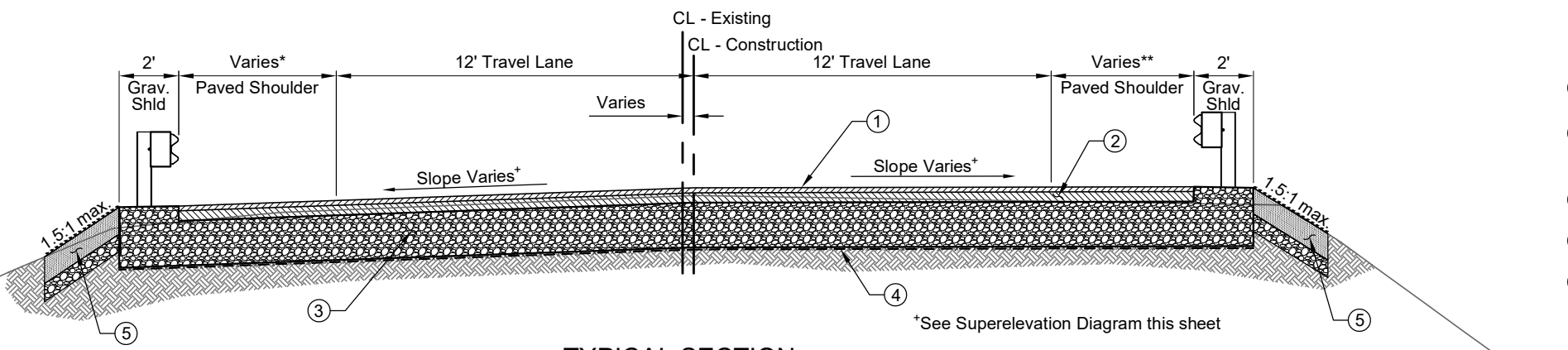
BRIDGE NO:	0002-0306	DATE:	1/13/2025
PROJECT NO:	CB2102		
TRS:	T. 12 S., R. 04 W., SECTION 17, W.M.		
DESIGNED BY:	K. Groom	CHECKED BY:	A. Potts
DRAFTED BY:	K. Groom	REVIEWED BY:	D. Malone

OWL CREEK - PEORIA ROAD
BRIDGE REPLACEMENT

LINN COUNTY
2025

TRAFFIC CONTROL PLAN	
SCALE: no scale	SHEET RD-01

1/13/2025 11:16 AM

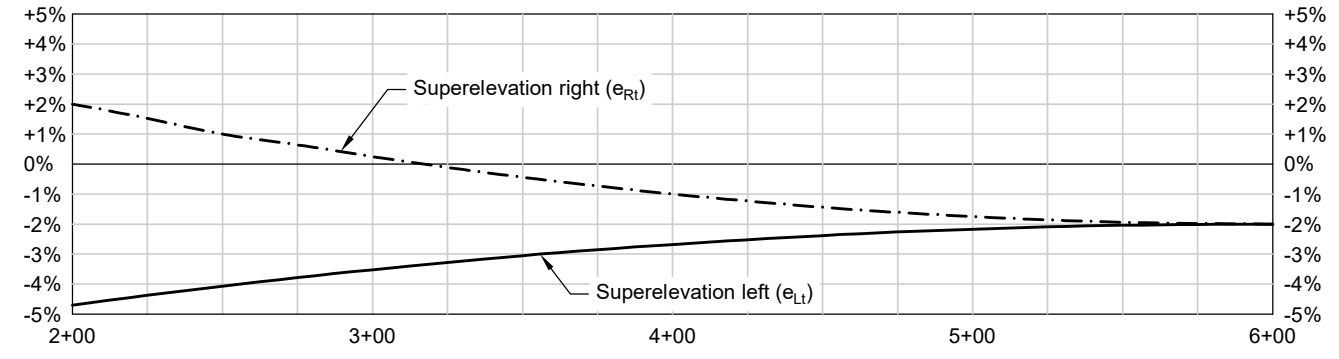


- ① Asphalt wearing course, Level 3, 1/2" dense graded ACP, 2" nominal thickness.
- ② Asphalt base course, Level 3, 1/2" dense graded ACP, 4" nominal thickness. Place in 2" maximum lift thickness.
- ③ Aggregate base course, 3/4" - 0, 18" thickness.
- ④ Subgrade geotextile, extend to edge of gravel shoulder.
- ⑤ 2'-6" wide filter strip. See Sht RD-06 for details.

***Left Shoulder**
 STA: 2+00.00 to STA: 2+67.50, 4.6' to 6.3'
 STA 2+67.50 to STA 3+08.90, 6.3' to 5.0'
 STA 3+08.90 to STA: 3+52.20, 5.0'
 STA: 4+48.80 to STA: 4+92.10, 5.0'
 STA 4+92.10 to STA 5+31.30, 5.0' to 6.2'
 STA 5+31.30 to STA: 6.00.00, 6.2' to 5.0'

TYPICAL SECTION
 NO SCALE
 STA: 2+00.00 TO STA: 3+52.20
 STA: 3+52.20 TO STA: 4+48.80 (Structure and bridge approach slabs)
 STA: 4+48.80 TO STA: 6+00.00

****Right Shoulder**
 STA: 2+00.00 to STA: 2+21.00, 2.8'
 STA 2+21.00 to STA: 3+08.90, 2.8' to 5.0'
 STA: 3+08.90 to STA: 3+52.20, 5.0'
 STA: 4+48.80 to STA: 4+92.10, 5.0'
 STA: 4+92.10 to STA: 5+31.30, 5.0' to 6.2'
 STA: 5+31.30 to STA: 6+00.00, 6.2' to 2.8'



$$e_{Lt} = -0.16875(Sta - 2.00)^2 + 1.35(Sta - 2.00) - 4.70$$

$$e_{Rt} = 0.25(Sta - 2.00)^2 - 2.00(Sta - 2.00) + 2.00$$
 Stations given in decimal format (ex. Station 4+25 = 4.25)

SUPERELEVATION DIAGRAM
 NO SCALE

WARNING
 If this bar does not measure 1" then drawing is not to scale

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 Digitally signed by Kevin M. Groom
 Date: 2025.01.13 11:30:47-08'00'
 OREGON
 JULY 19, 1994
 KEVIN M. GROOM
 Expires: 6/30/25



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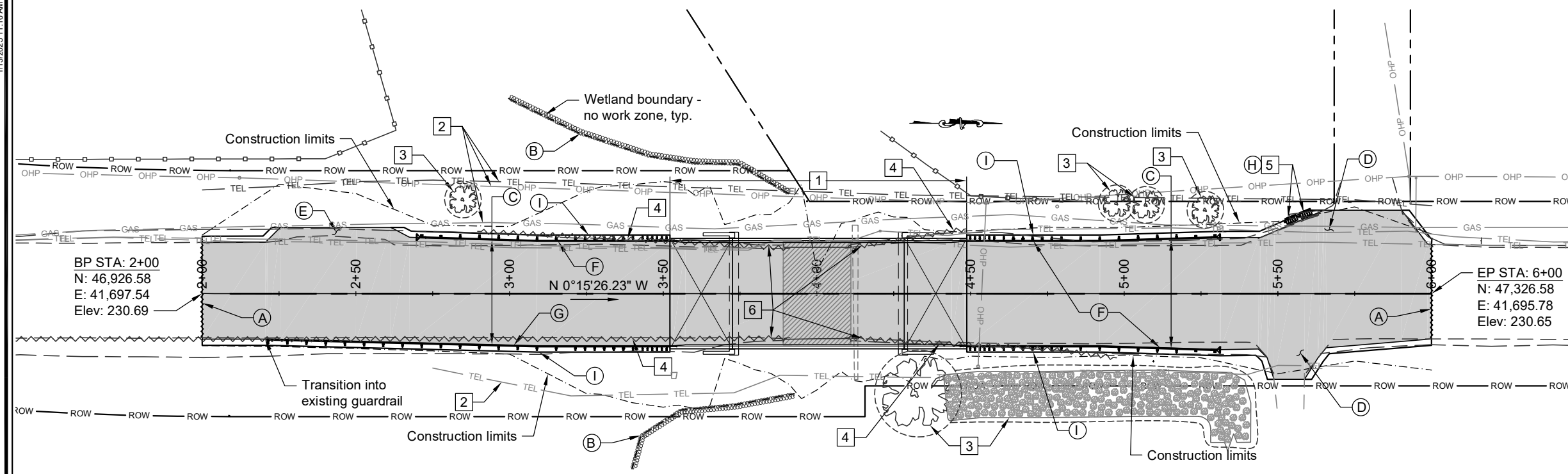
OWL CREEK - PEORIA ROAD
 BRIDGE REPLACEMENT
 LINN COUNTY
 2025

TYPICAL SECTION AND
 SUPERELEVATION DIAGRAM
 SCALE: None
 SHEET RD-02

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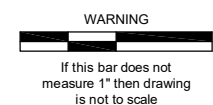
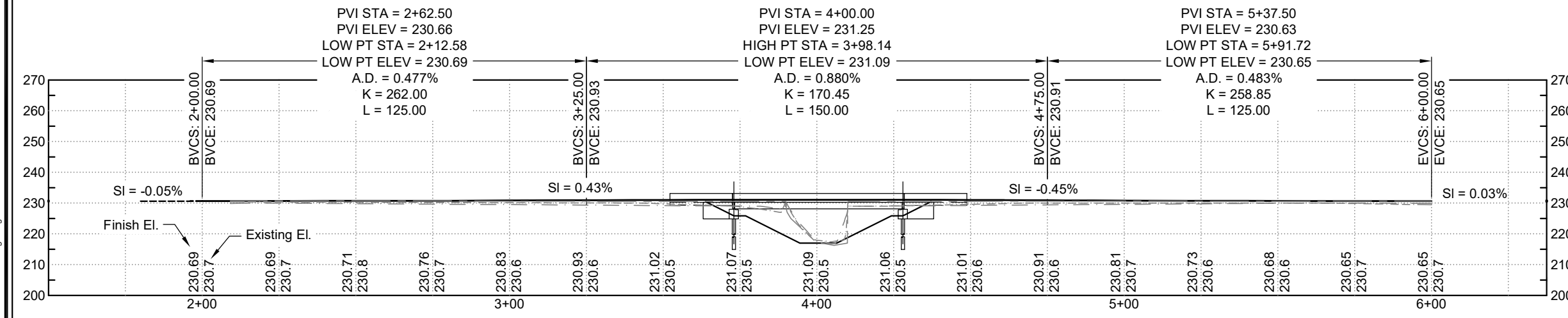
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- 1 Remove existing bridge and construct County Bridge Number 0002-0306 on Peoria Road over Owl Creek. See Bridge Plans for details.
 - 2 Retain and protect existing utilities.
 - 3 Retain and protect existing trees and hedge at STA: 2+85 LT, STA: 4+32 RT, STA: 4+95 LT, STA: 5+07 LT, STA: 5+26 LT, and STA: 4+40 to STA: 5+40 RT.
 - 4 Remove existing guardrail from STA: 2+89 to STA 3+89 LT, from STA: 2+20 to STA: 3+89 RT, from STA: 4+11 to STA: 4+49 LT, and from STA: 4+11 to STA: 4+98 RT
 - 5 Remove and protect existing mailboxes at STA: 5+85 LT.
 - 6 Remove existing object marker signs and bridge ID signs near existing bridge corners. Return bridge ID signs to on-site County Inspector.
- (A) Provide full depth vertical sawcut full width of existing pavement at STA: 2+00 and STA: 6+00.
- (B) Install no work-zone fencing from STA: 3+00 to STA: 3+90 LT, and from STA: 3+40 to STA: 3+95 RT.
- (C) Construct roadway section from STA: 2+00 to STA: 3+52 and from STA: 4+48 to STA: 6+00. See Sheet RD-02 for Typical Section.
- (D) Construct 10' asphalt pavement feather at driveway and field accesses at STA: 5+55 RT and STA: 5+70 LT.
- (E) Construct 3' asphalt wingout at STA: 2+40 LT.
- (F) Install thrie-beam terminal connector, guardrail transition, 12.5' of Type 3 guardrail, 12.5' of Type 2A guardrail, and energy absorbing guardrail terminal (Test Level 3) with $W = 1'$ from the end of the bridge rail, per standard drawings BR203, RD402, RD404, RD406, RD409, RD410, RD416, RD417, RD420 and RD442. All guardrail to be constructed with 8'-0" long steel posts. All non-steel blocks shall be plastic from the ODOT QPL List.
- (G) Install thrie-beam terminal connector, guardrail transition, 12.5' of Type 3 guardrail, Type 2A guardrail for required length, and 25' Midwest Guardrail System Height Conversion section to tie into existing guardrail near STA: 2+20 per standard drawings BR203, RD402, RD404, RD406, RD409, RD410, RD416, RD417, RD420, RD442 and RD481. All guardrail to be constructed with 8'-0" long steel posts. All non-steel blocks shall be plastic from the ODOT QPL List.
- (H) Install existing mailboxes on 2 new multiple mailbox supports per RD100 at STA: 5+57 LT.
- (I) Construct 2' gravel shoulders from STA: 2+00 to STA: 3+52 LT and RT, and from STA: 4+49 to STA: 6+00 LT and RT.

PEORIA ROAD PROFILE



REGISTERED PROFESSIONAL ENGINEER
17206PE
Digitally signed by Kevin M. Groom
Date: 2025.01.13 11:30:47-08'00'
OREGON
JULY 19, 1994
KEVIN M. GROOM
Expires: 6/30/25



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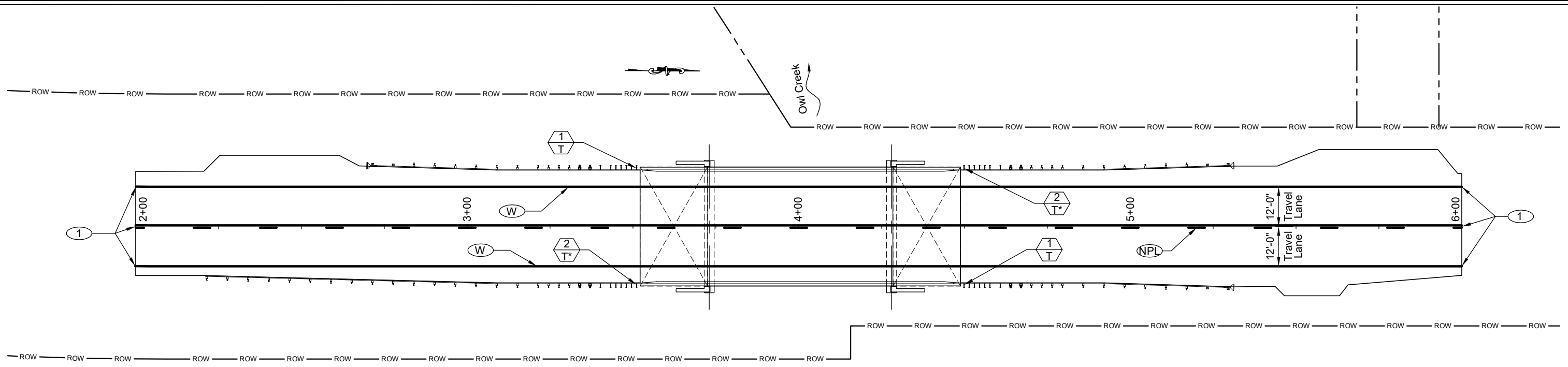
OWL CREEK - PEORIA ROAD
BRIDGE REPLACEMENT

LINN COUNTY
2025

PLAN AND PROFILE

SCALE: 1" = 40'
SHEET RD-03

1/13/2025 11:16 AM



SIGNING AND STRIPING PLAN
1" = 30'-0"

- ① Match existing striping
- NPL No-Pass Left - 4" yellow lines shown thus (see TM500)
- W 4" White line shown thus (see TM500)

Note:
The Contractor is to remove any existing striping or permanent markings that conflict with the new striping or markings.



Sign 1
Type: OM-3L
(12" x 36")



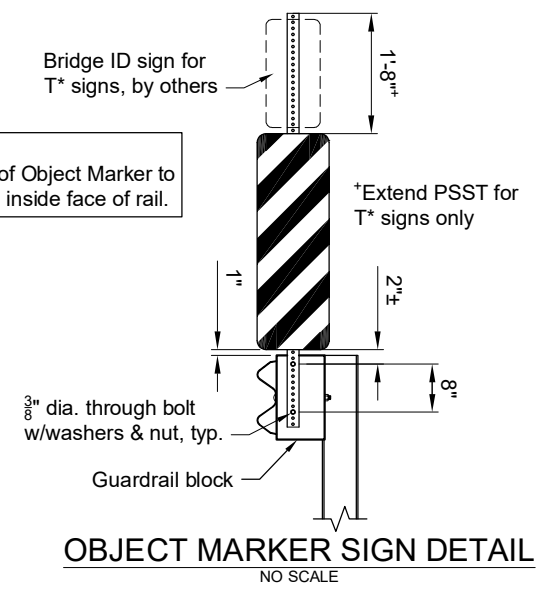
Sign 2
Type: OM-3R
(12" x 36")

- Signing Notes:**
- The locations of sign installations shown are approximate with exact locations to be determined in the field.
 - Install signs per ODOT Standard Drawings TM200, TM201, TM676, TM681, TM687 and the Object Marker Sign Detail shown on this sheet.
 - All signs shall meet the requirements of the most current Manual on Uniform Traffic Control Devices (MUTCD).

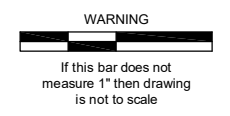
Install Sign (N) and Support (T)

Legend:
N = Sign Number
T = Perforated Steel Square Tube (PSST)
T* = Perforated Steel Square Tube (PSST) w/1'-8" extending above sign (N)

Note:
Inside edge of Object Marker to be flush with inside face of rail.



OBJECT MARKER SIGN DETAIL
NO SCALE



REGISTERED PROFESSIONAL ENGINEER
17206PE

Digitally signed by Kevin M. Groom
Date: 2025.01.13 11:30:47-08'00'

OREGON
JULY 19, 1994
KEVIN M. GROOM

Expires: 6/30/25



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OWL CREEK - PEORIA ROAD
BRIDGE REPLACEMENT

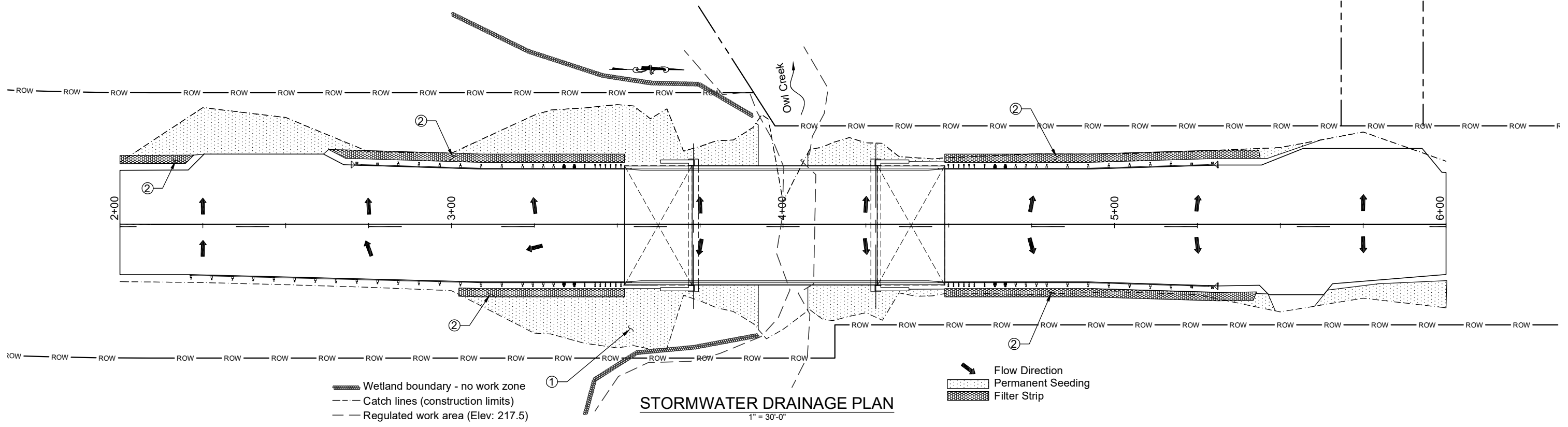
LINN COUNTY
2025

SIGNING AND STRIPING PLAN

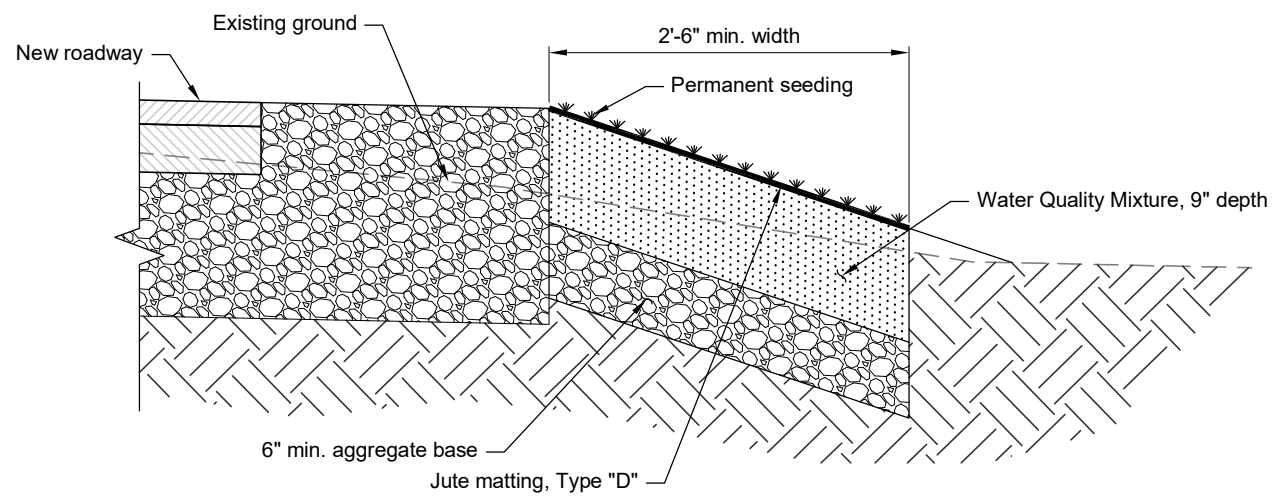
SCALE: as shown
SHEET RD-05

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1/13/2025 11:17 AM



- ① Install Permanent Seeding per Sheet RD-04
- ② Install filter strips, see detail this sheet



WARNING
If this bar does not measure 1" then drawing is not to scale

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WAYNE E. MINK, P.E.
COUNTY ENGINEER
DAINEAL MALONE, P.E.

DATE:	REVISION:	BY:	BRIDGE NO:	DATE:
			0002-0306	1/13/2025
			PROJECT NO: CB2102	
			TRS: T. 12 S., R. 04 W., SECTION 17, W.M.	
			DESIGNED BY: K. Groom	CHECKED BY: A. Potts
			DRAFTED BY: K. Groom	REVIEWED BY: D. Malone

OWL CREEK - PEORIA ROAD
BRIDGE REPLACEMENT

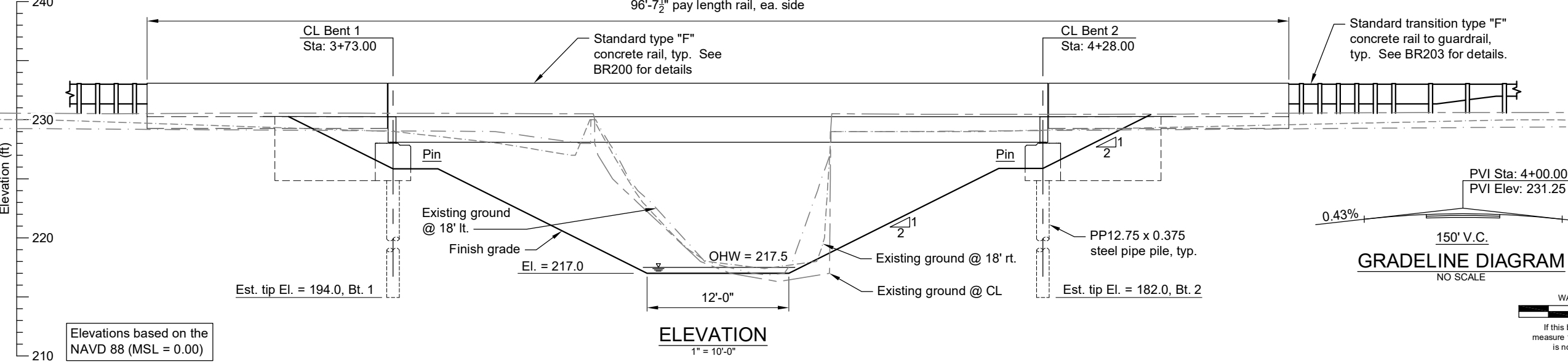
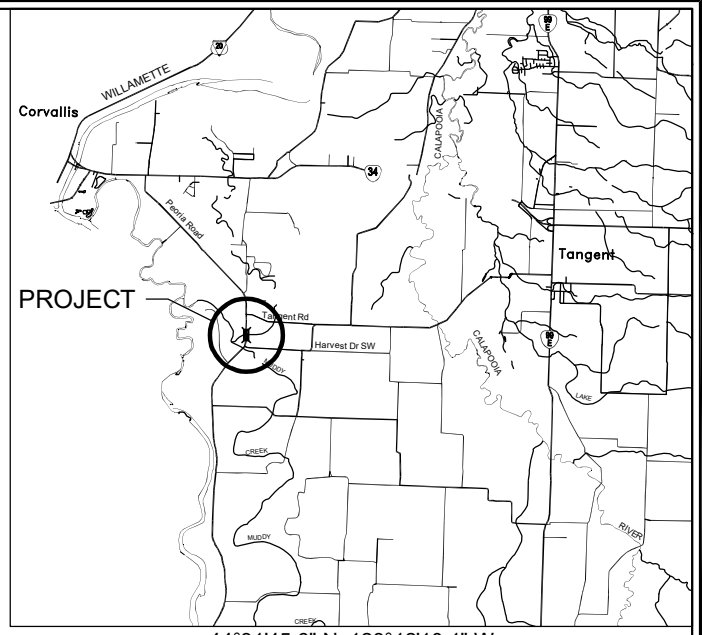
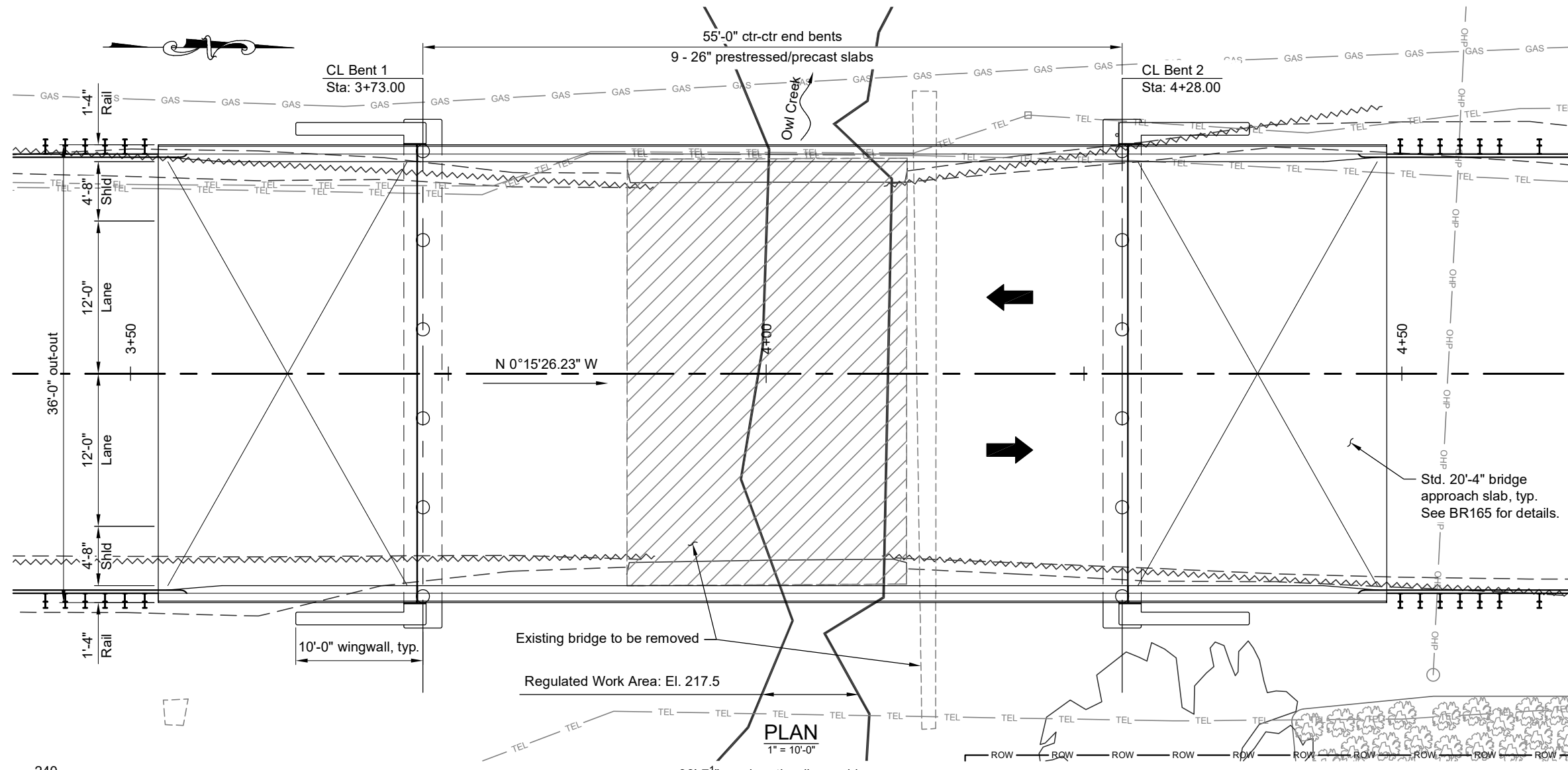
LINN COUNTY
2025

SCALE: as shown

SHEET RD-06

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OWL CREEK - PEORIA ROAD
 BRIDGE REPLACEMENT

LINN COUNTY
 2025

PLAN AND ELEVATION

SCALE: as shown

SHEET BR-01

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General Notes:

All materials and workmanship shall conform to the 2024 Oregon Standard Specifications for Construction, the Project Special Provisions, and the current edition of The General Conditions for Construction for the Linn County Road Department.

Bridge is designed in accordance with the 9th edition of the AASHTO LRFD Bridge Design Specifications with an allowance for the wearing surface shown on the plans, 25 psf for future wearing surface, and all the following live loads:

Service and Strength-I Limit States:

HL-93: Design truck (or trucks per LRFD 3.6.1.3) or the design tandem, and the design lane load

Strength-II Limit State:

ODOT Permit Vehicle Type OR-STP-4E
 ODOT Permit Vehicle Type OR-STP-5BW

Seismic design is performed in accordance with the "AASHTO LRFD Bridge Design Specifications" ("AASHTO Guide Specifications for LRFD Seismic Bridge Design") as modified by the "ODOT Bridge Design Manual" for 500 and 1000-year criteria. The Horizontal Peak Ground Acceleration Coefficient (PGA) for the 1000 year (No Collapse) return period is 0.36g based on the 2014 USGS Seismic Hazard Maps. The bridge site is defined as a Site Class D with a site factor (F_{pga}) of 1.33.

Provide all reinforcing steel according to ASTM Specification A706 or ASTM A615, Grade 60. Provide welded or field bent bars according to ASTM Specification A706. Splice reinforcing steel at alternate bars, staggered at least one splice length or as far as possible, unless shown otherwise. Use the following splice lengths, unless shown otherwise:

SPLICE LENGTH (CLASS B)*												
Bar Size	#3	#4	#5	#6	#7	#8	#9	#10	#11	#14	#18	
Uncoated	1'-4"	1'-9"	2'-2"	2'-7"	3'-0"	3'-5"	3'-10"	4'-4"	4'-11"	Not Permitted		

*Increase the splice lengths by ALL the applicable percentages:

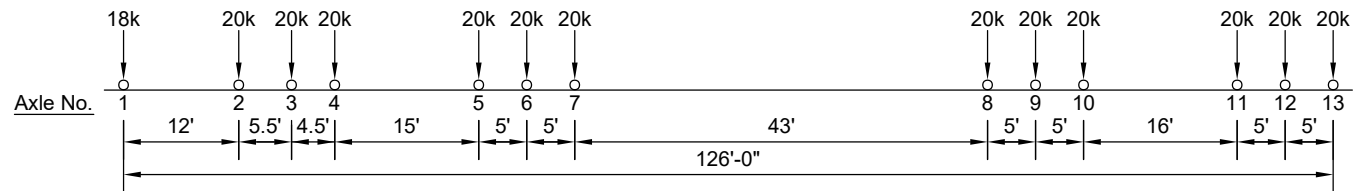
- 40% for locations with 12 inches or more of fresh concrete placed below.
- 30% for areas with more than 50% of bars spliced in one region.

Place reinforcing bars a minimum of 2-inches clear of the nearest face of concrete, unless shown otherwise.

Provide Class 3300 - 1" or $\frac{3}{4}$ " concrete for all concrete not otherwise specified.

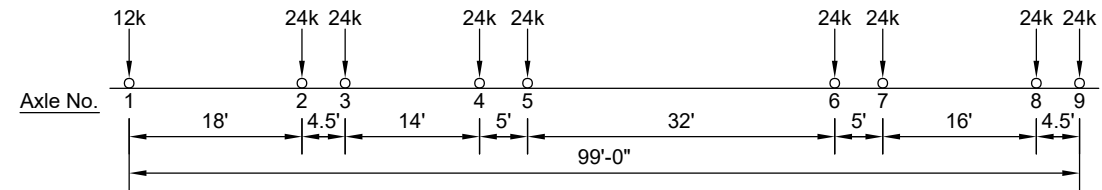
Provide concrete and prestressing steel in precast prestressed slabs in accordance with detail plans.

Provide bridge rail materials according to the ODOT Standard Drawings referenced.



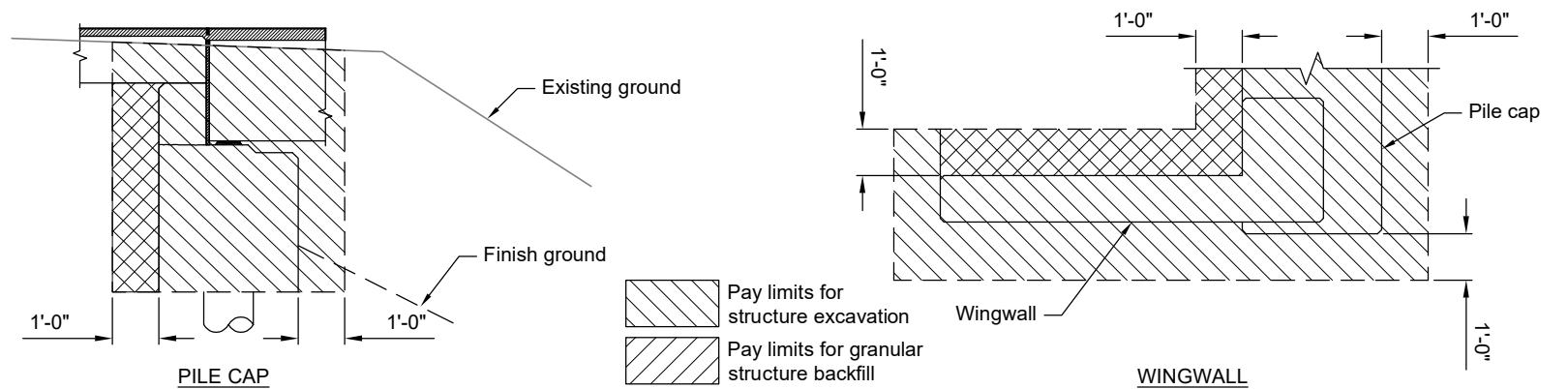
ODOT PERMIT VEHICLE TYPE OR-STP-4E

NO SCALE



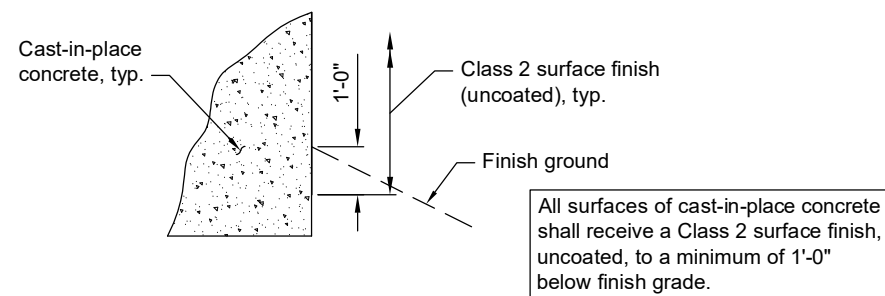
ODOT PERMIT VEHICLE TYPE OR-STP-5BW

NO SCALE



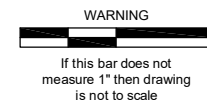
STRUCTURE EXCAVATION AND BACKFILL PAY LIMITS

NO SCALE



CONCRETE FINISH DIAGRAM

NO SCALE



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PROJECT NO: CB2102	
TRS: T. 12 S., R. 04 W., SECTION 17, W.M.	
DESIGNED BY: K. Groom	CHECKED BY: A. Potts
DRAFTED BY: K. Groom	REVIEWED BY: D. Malone

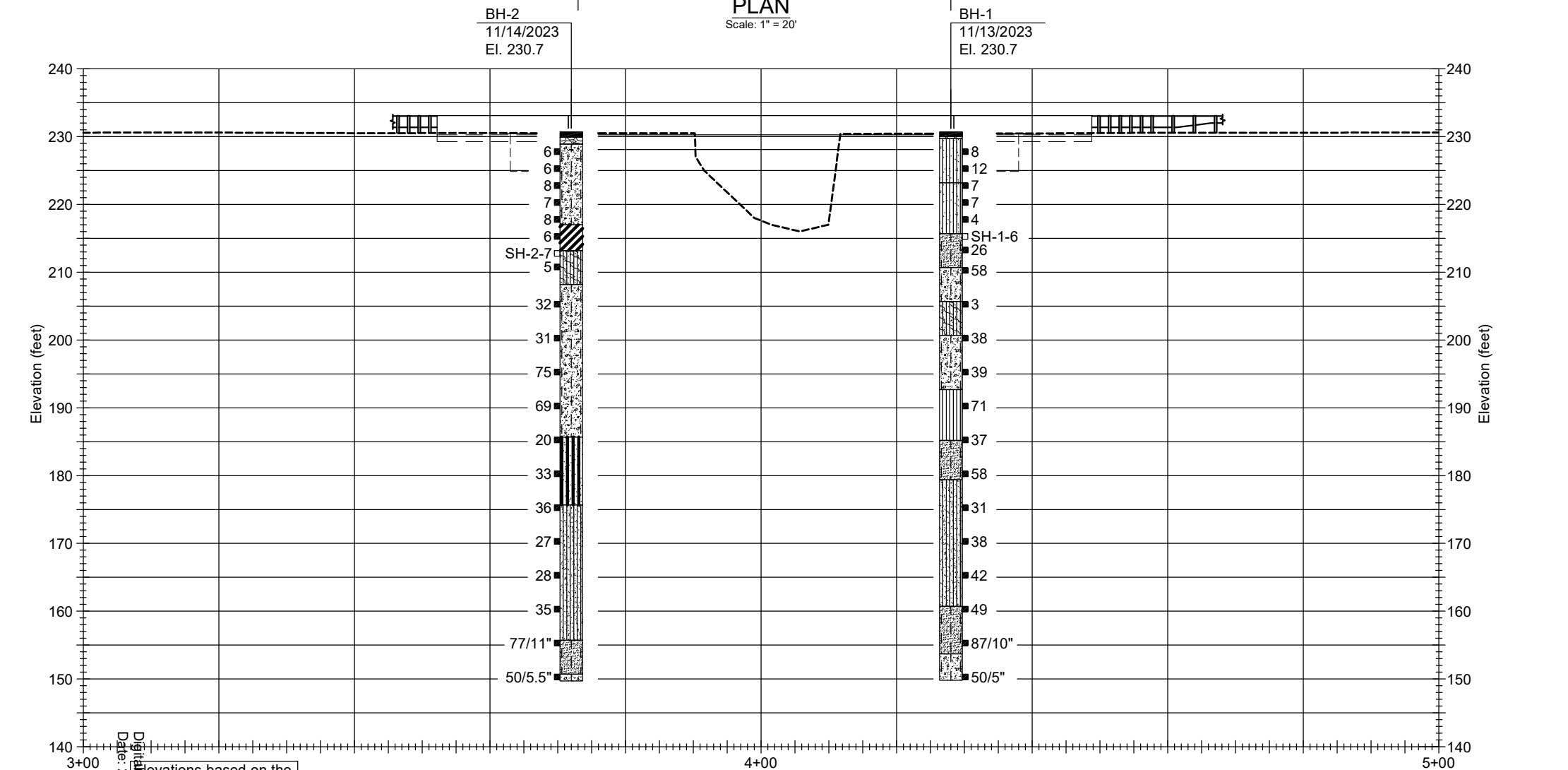
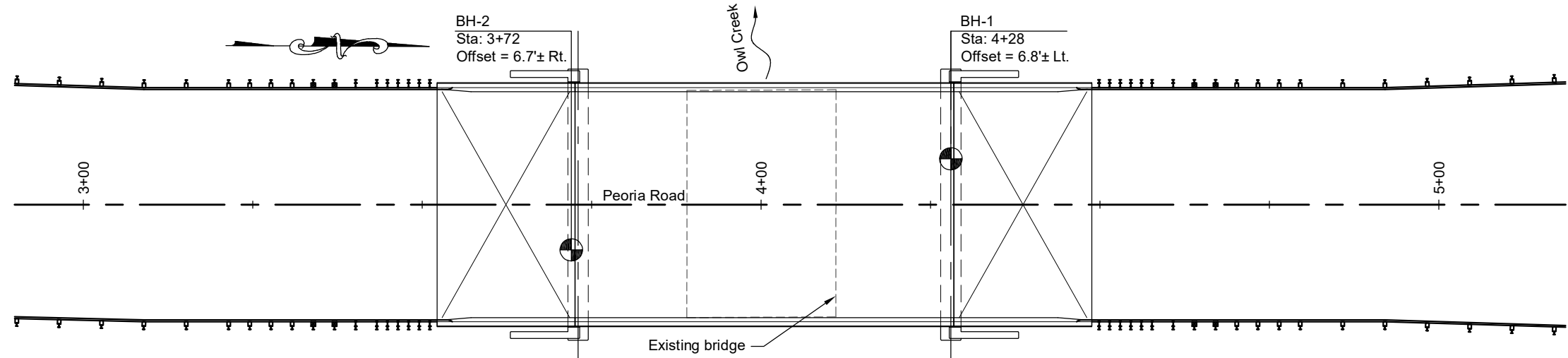
OWL CREEK - PEORIA ROAD
 BRIDGE REPLACEMENT
 LINN COUNTY
 2025

GENERAL NOTES	
SCALE: None	SHEET BR-02

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UNIT DESCRIPTIONS

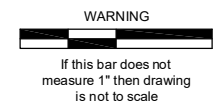
- ASPHALTIC CONCRETE
- CRUSHED GRAVEL (GW); grey, moist, dense, ±3/4 inch minus subangular to subrounded gravel, (base rock).
- Sandy SILT (ML) and SILT, some clay and sand (ML); brown, blue-grey, and blue-grey to grey, non-plastic to medium plasticity, damp to wet, soft to hard, fine sand, (alluvium).
- Silty SAND (SM) and SAND, some silt (SP-SM); brown, blue-grey to grey, and grey, non-plastic to low plasticity silt, moist to wet, very loose to very dense, fine to medium sand, (alluvium).
- Silty sandy GRAVEL (GM); brown, blue-grey, and brown and iron-stained, non-plastic to low plasticity silt, moist to wet, loose to very dense, fine to coarse sand, fine to coarse subangular to rounded gravel, (alluvium).
- Sandy SILT, some organics (ML); dark brown, grey, and brown and iron-stained, non-plastic to low plasticity, moist to wet, soft to medium stiff, fine sand, organics consist of wood debris and wood fibers, (alluvium).
- SILT (ML); brown and light brown to blue-grey, low plasticity, damp, stiff to hard, (alluvium).
- Gravelly silty SAND (SM); blue grey to grey, non-plastic to low plasticity silt, moist, very dense, fine to coarse sand, fine subrounded gravel, (alluvium).
- CLAY (CH); dark grey, medium to high plasticity, moist, medium stiff, (alluvium).
- Sandy clayey SILT (MH); grey to blue-grey, medium to high plasticity, moist, very stiff to hard, fine sand, (alluvium).

LEGEND

- = Geotechnical Boring (BH)
- = Standard Penetration Test
- = N value
- 50/#" or 87/#" = SPT Test Refusal Length
- SH-3-2 = Shelby Tube Sample

General Notes:

1. Geotechnical data shown on this drawing are a consolidation of information and/or revision in terminology from the geotechnical boring logs. More detailed subsurface data is available on the boring logs in the geotechnical report. The boring logs and geotechnical report are available upon request for review at the office of the Linn County Road Department. Contractor shall refer to the geotechnical report, boring logs, and information therein.
2. In accordance with ASTM D1586, N-values are reported for an interval of 1-foot except as noted.
3. SPT samples retained using a hammer efficiency of 91%.
4. Refer to ODOT Soil and Rock Classification and Logging - Chapter 5 of the 2023 GDM for a description of the terms used on this sheet.



Foundation Engineering, Inc.
Professional Geotechnical Services

820 N.W. CORNELL AVENUE
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BUS. (541) 757-7645



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			TRS: T. 12 S., R. 04 W., SECTION 17, W.M.	
			DESIGNED BY: W. Nickels	CHECKED BY: Brooke Running
			DRAFTED BY: K. Groom	REVIEWED BY: M. McAdams

OWL CREEK - PEORIA ROAD
BRIDGE REPLACEMENT

LINN COUNTY
2025

FOUNDATION DATA SHEET

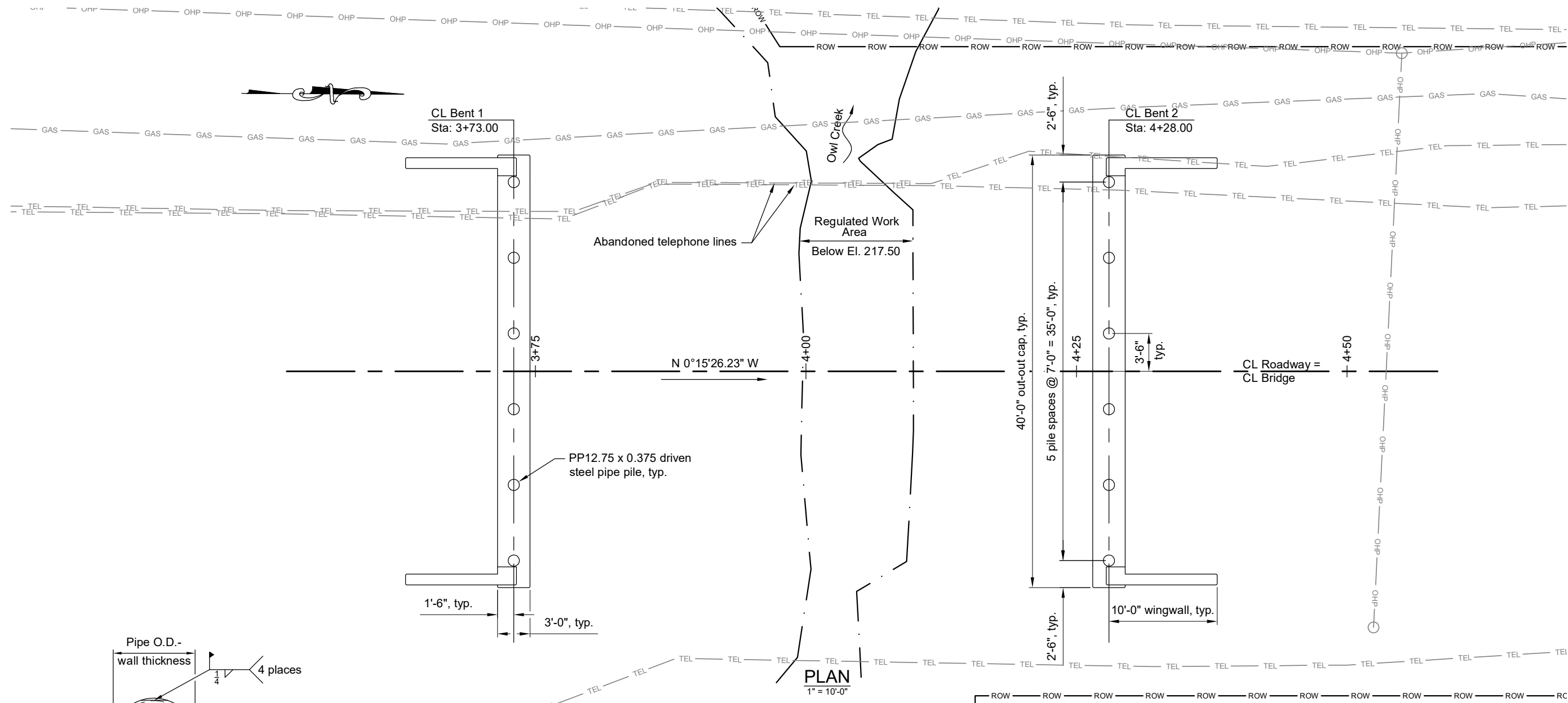
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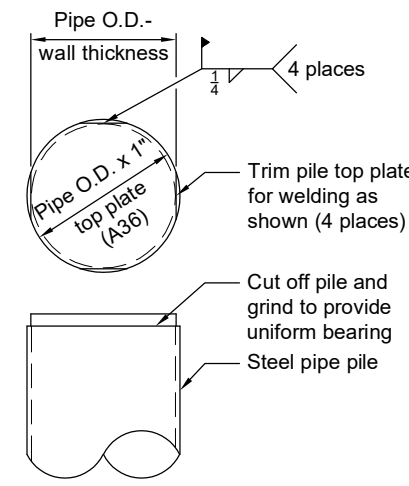


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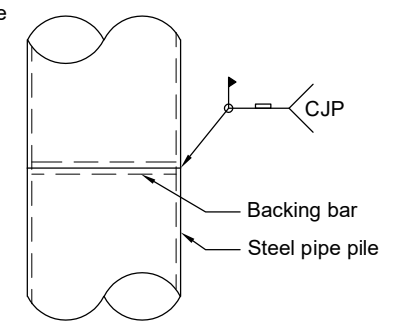
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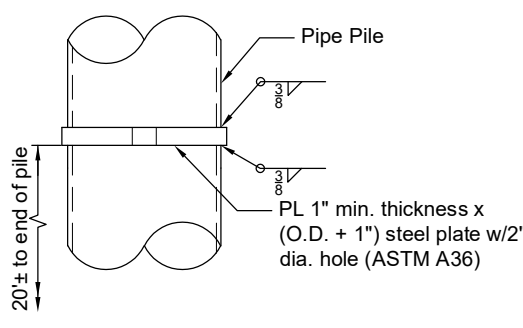
PLAN
1" = 10'-0"



TOP OF PIPE PILE



PIPE PILE SPLICE

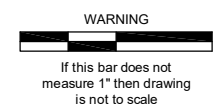


PIPE PILE PLUG

STEEL PIPE PILE DETAILS

NO SCALE

Foundation General Notes:
 All piling shall be PP 12.75 x 0.375 conforming to ASTM Specification A252, Grade 3 and driven open ended with inside-fitting cutting shoes to a nominal resistance of 465 kips per pile for Bent 1, and 450 kips per pile for Bent 2. Minimum pile tip elevation is 205.0 feet for Bent 1, and 200.0 feet for Bent 2. The estimated pile tip elevations are 194.0 and 182.0 feet for Bents 1 & 2, respectively. Drive piling to the specified nominal resistance using driving criteria developed from the FHWA Gates Equation.



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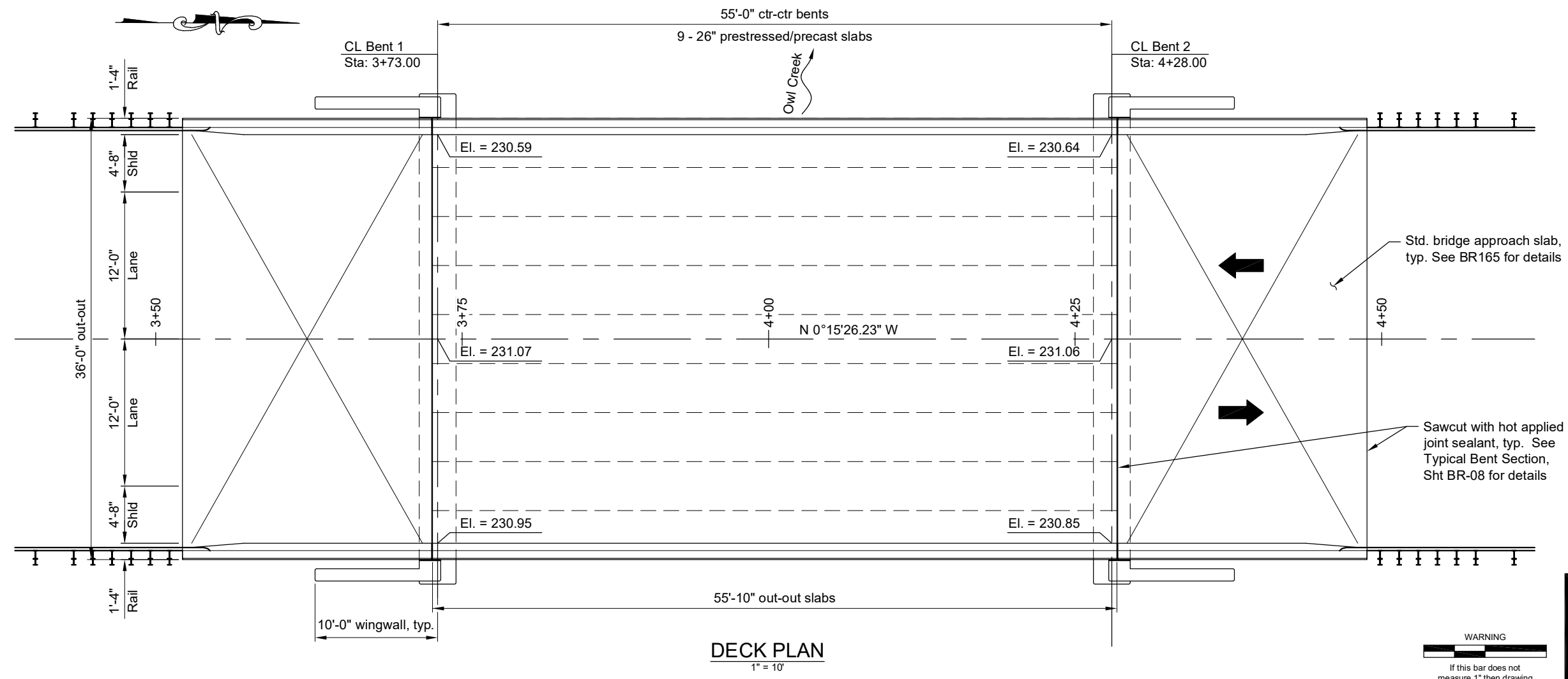
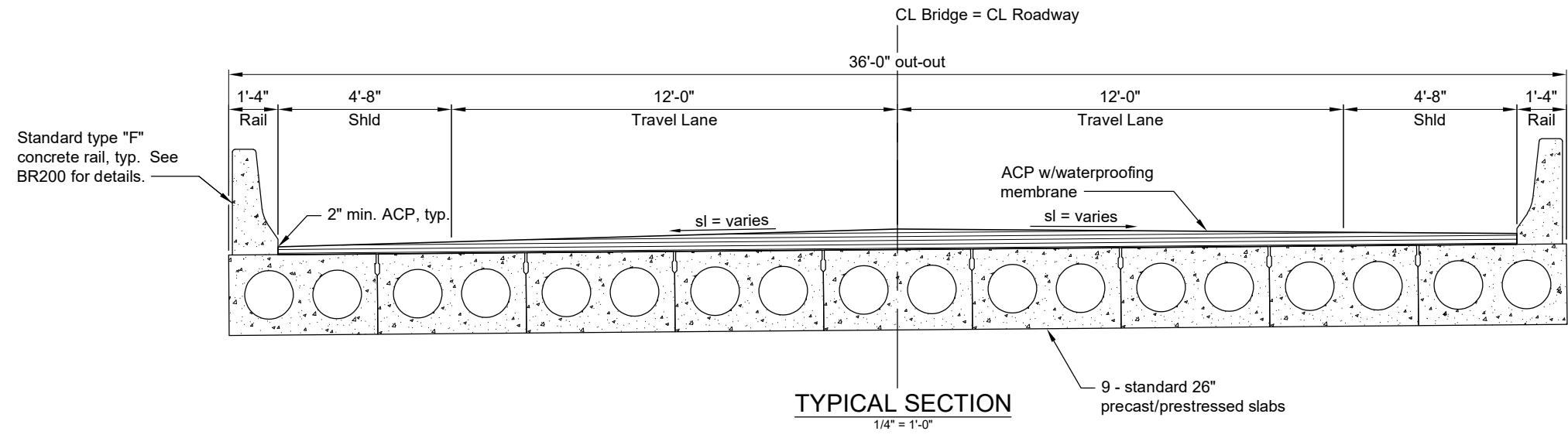
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DRAFTED BY:	K. Groom	REVIEWED BY:	D. Malone

OWL CREEK - PEORIA ROAD
 BRIDGE REPLACEMENT
 LINN COUNTY
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FOUNDATION PLAN AND DETAILS
 SCALE: as shown
 SHEET BR-04

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WARNING
If this bar does not
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is not to scale

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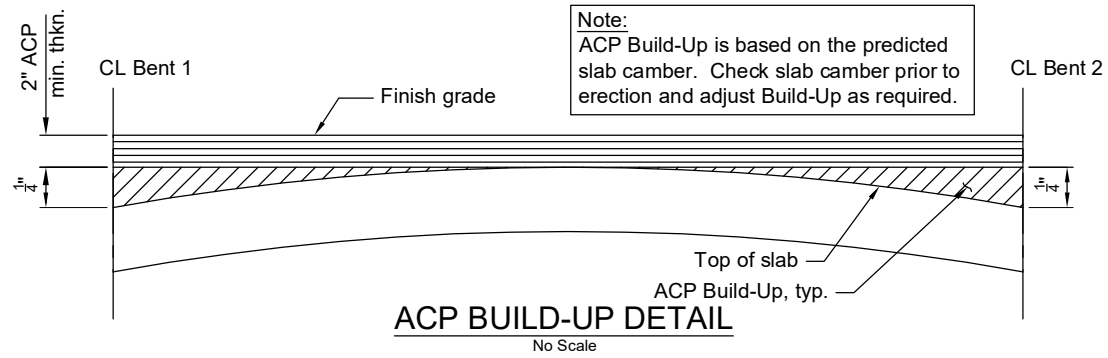
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LINN COUNTY
2025

TYPICAL SECTION AND DECK PLAN
SCALE: as shown
SHEET BR-05

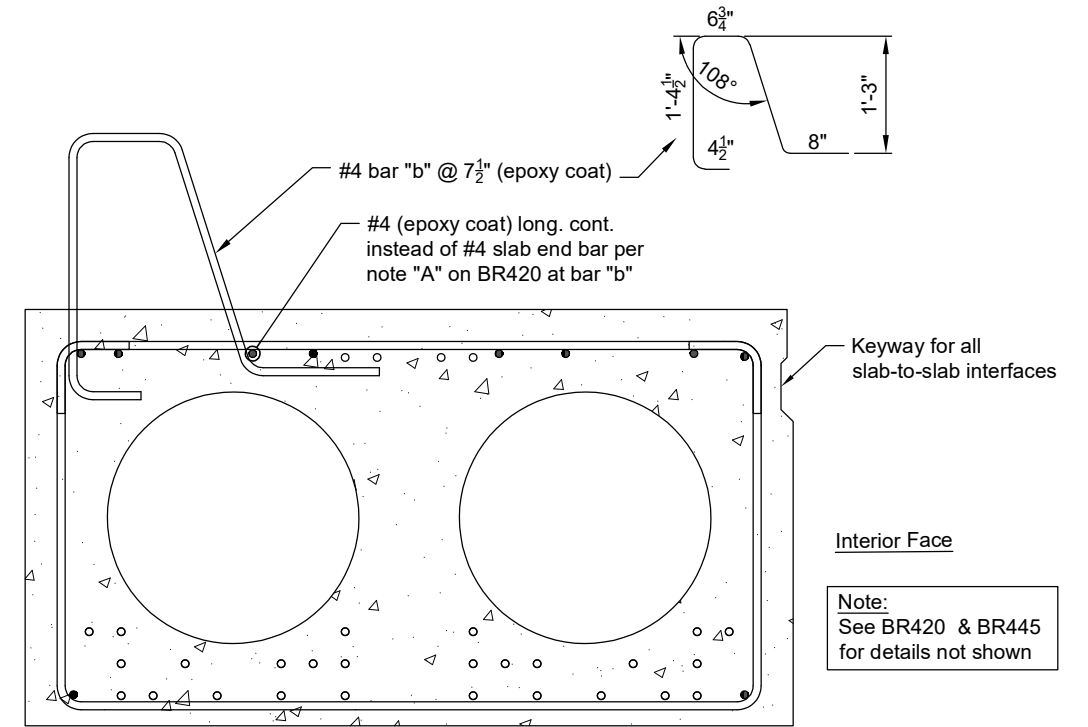
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26 - INCH STANDARD PRECAST PRESTRESSED SLAB SCHEDULE																			
No. of slabs required	Span No.	Horizontal length o-o at slab CL (after shortening)	Span ctr.-ctr. bearing along slab CL	Width, inches	Skew Angle, Deg.		Initial tension per strand, kips	Total strands	No. debonded strands	Distance "Yc" to strand c.g. at midspan, inches	Distance "Yu" to strand c.g. at midspan subtracting top 4 strands, inches	Concrete Class, psi A = 3/4" or 1"	Minimum concrete strength at transfer of prestress, psi	Estimated initial strand stress loss, ksi	Estimated midspan deflection, inches				Estimated shortening 2 weeks after transfer of prestress, inches
					Back	Ahead									Upward at transfer of prestress	Upward 3 months after transfer of prestress (No SIDL)	Upward 5 years after transfer of prestress (No SIDL)	Instantaneous downward due to SIDL	
1	55'-10"	55'-0"	48.0	0	0	31.0	34	2	5.77	3.48	5,500	4,200	9.4	0.71	1.24	1.58	0.22	0.44	0.31

The superimposed dead load (SIDL) is 79.1 psf, which includes the initial wearing surface and bridge rails. The slabs are designed for an allowance for 2" of future wearing surface.



Exterior Face



26\"/>

WARNING
If this bar does not measure 1\"/>

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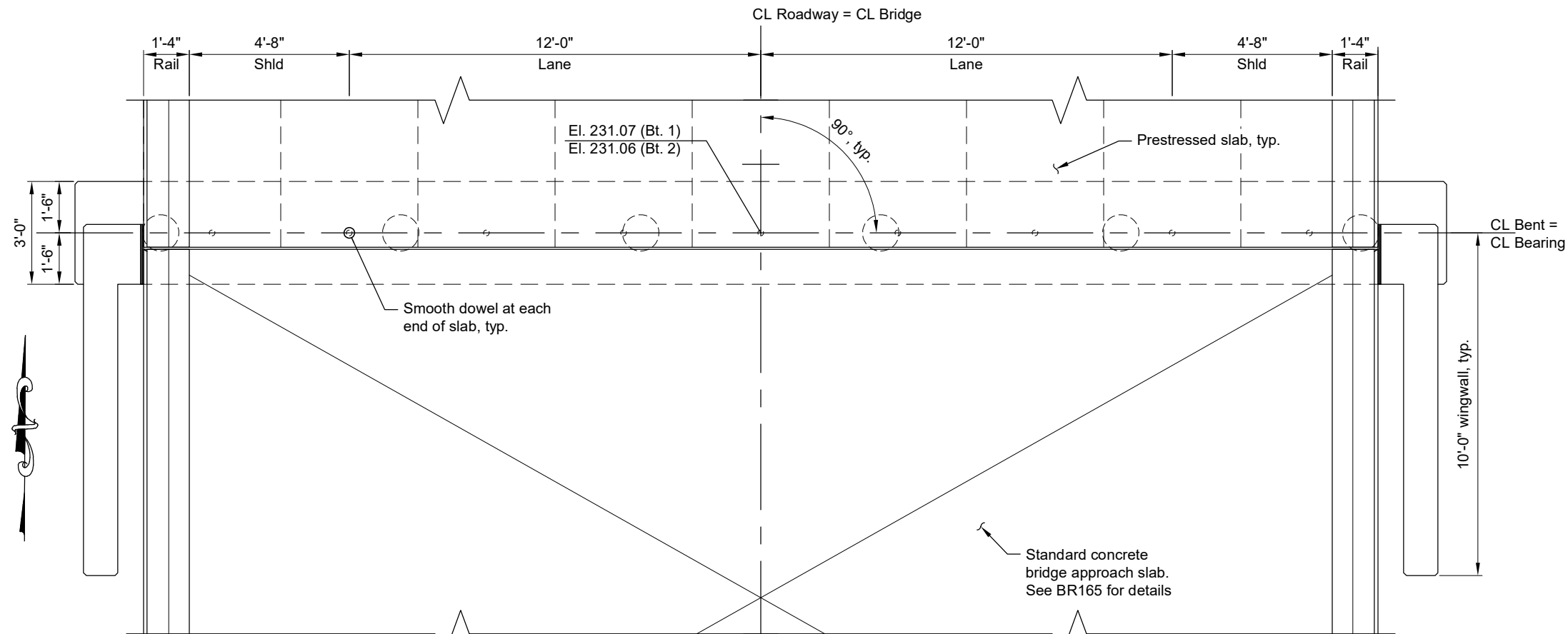
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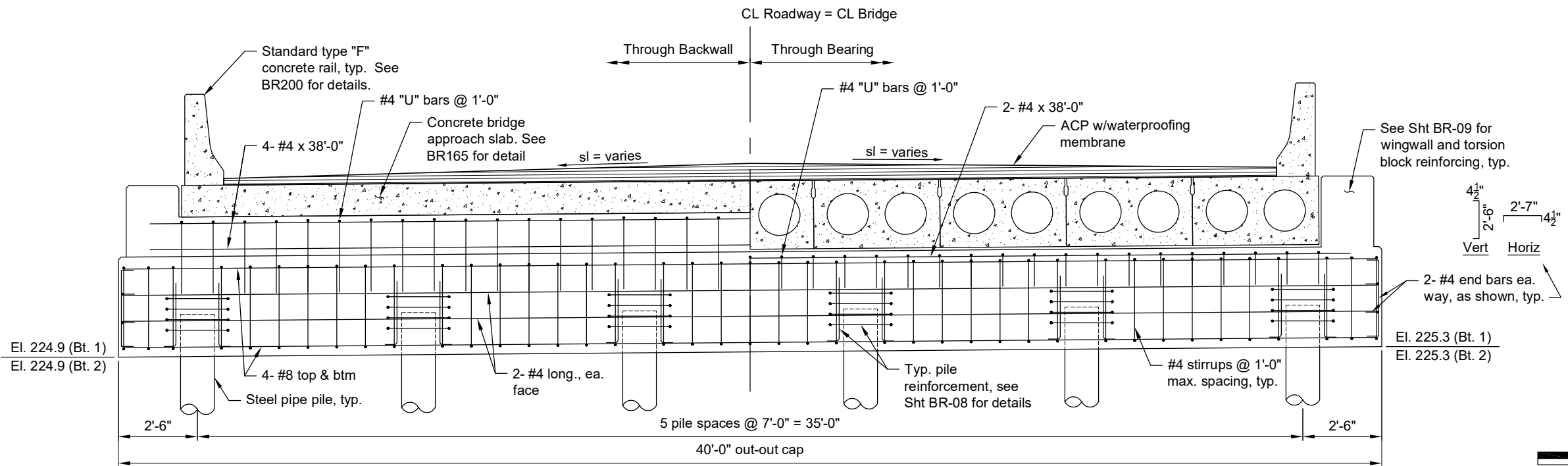
SLAB DETAILS	
SCALE: as shown	SHEET BR-06

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BENT 1 - PLAN (BENT 2 SIMILAR)
1/4" = 1'-0"



BENT 1 - ELEVATION (BENT 2 SIMILAR)
1/4" = 1'-0"

WARNING
If this bar does not measure 1" then drawing is not to scale

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OWL CREEK - PEORIA ROAD
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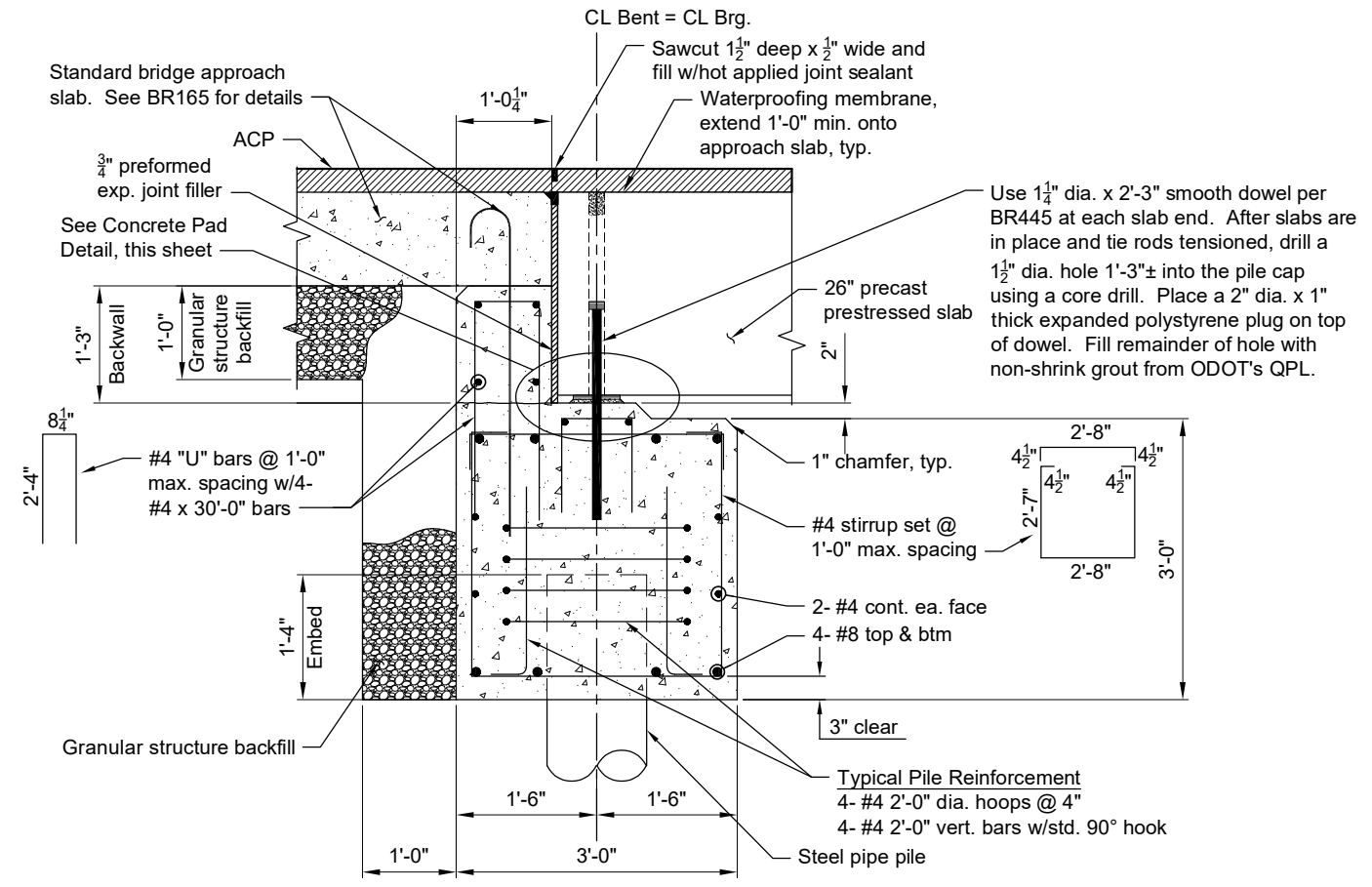
BENT 1 PLAN AND ELEVATION

SCALE: as shown

SHEET BR-07

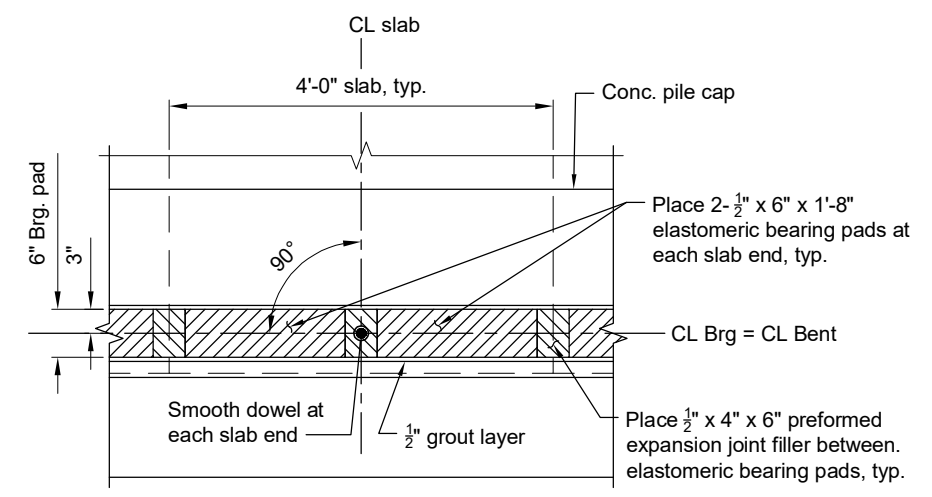
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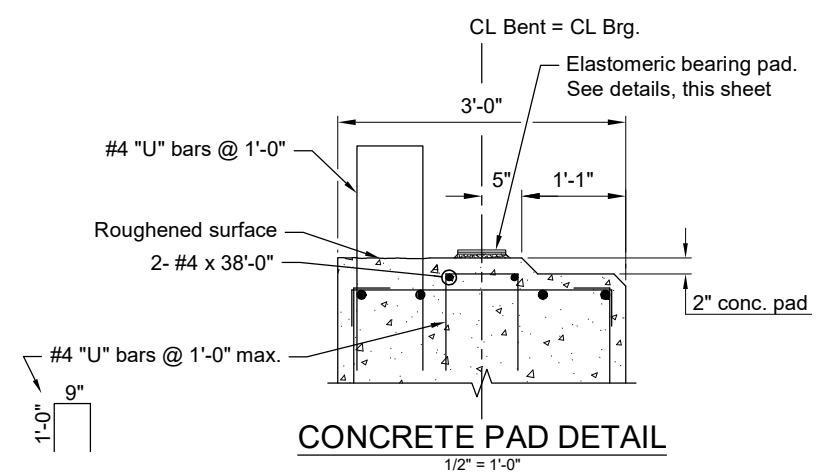


TYPICAL BENT SECTION
1/2" = 1'-0"

Slab Setting Procedure:
Form 2" concrete pad integrally with the pile cap. Place 1/2"± grout layer immediately before placing slabs. Place elastomeric bearing pads, preformed expansion joint filler and prestressed slabs before the grout is fully set to insure uniform bearing across full width of the slab. If uniform bearing is not achieved, lift slab and repeat procedure. Any excess grout protruding above bottom of bearing pads shall be removed immediately after placing slabs.



BEARING PAD DETAIL
1/2" = 1'-0"



CONCRETE PAD DETAIL
1/2" = 1'-0"

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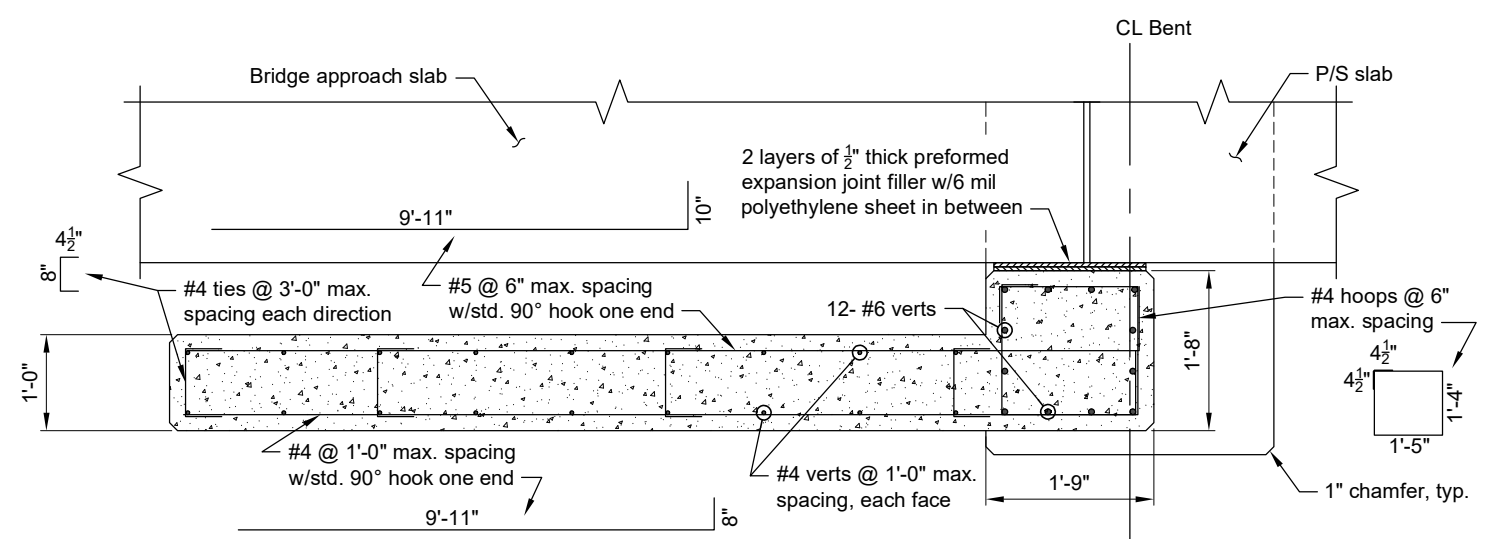
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LINN COUNTY
2025

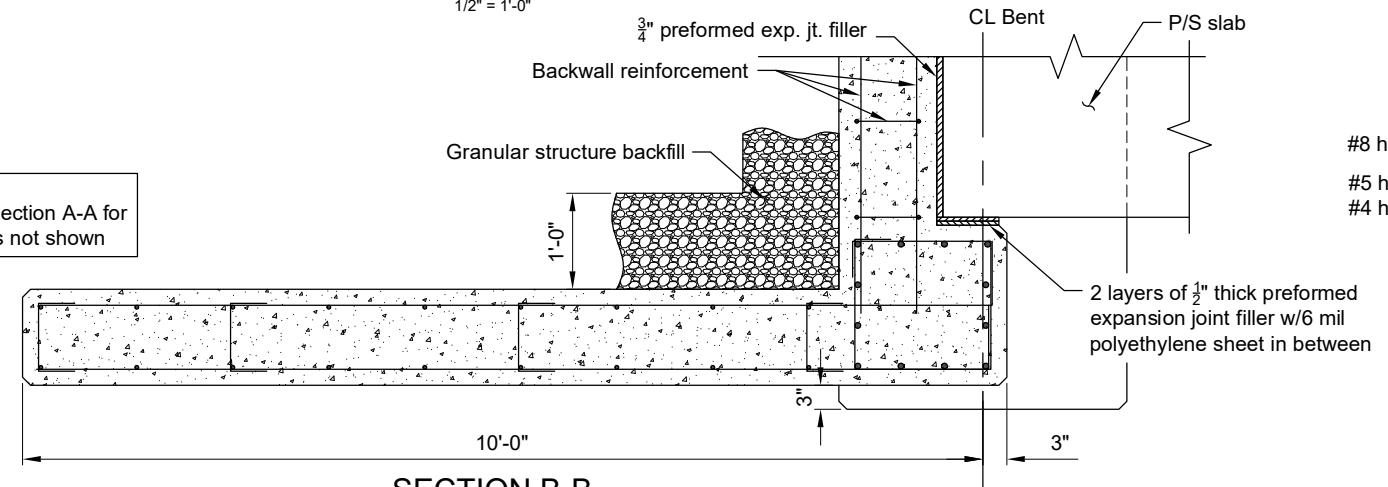
BENT DETAILS
SCALE: as shown
SHEET BR-08

1/13/2025 11:19 AM



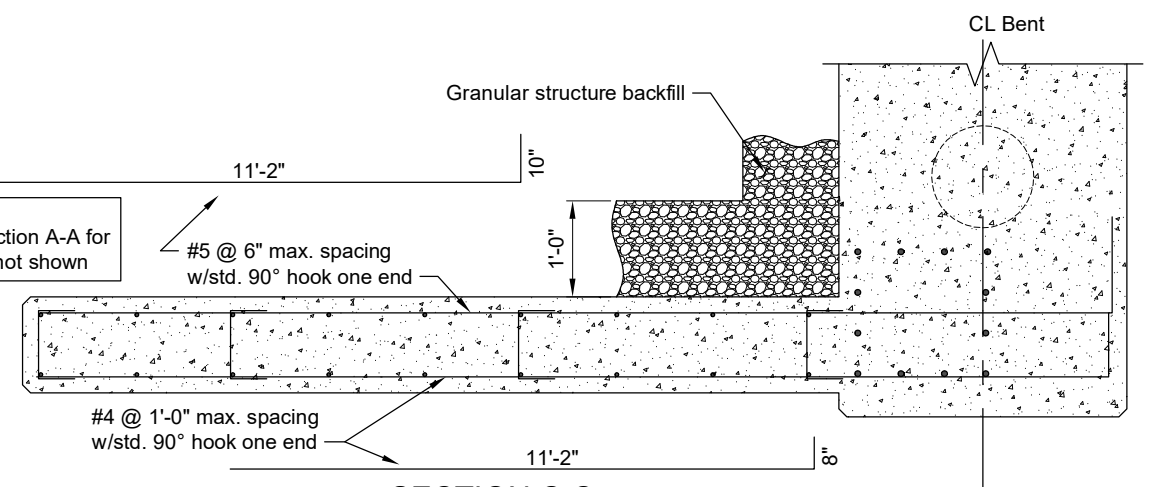
SECTION A-A
1/2" = 1'-0"

Note:
See Section A-A for
details not shown

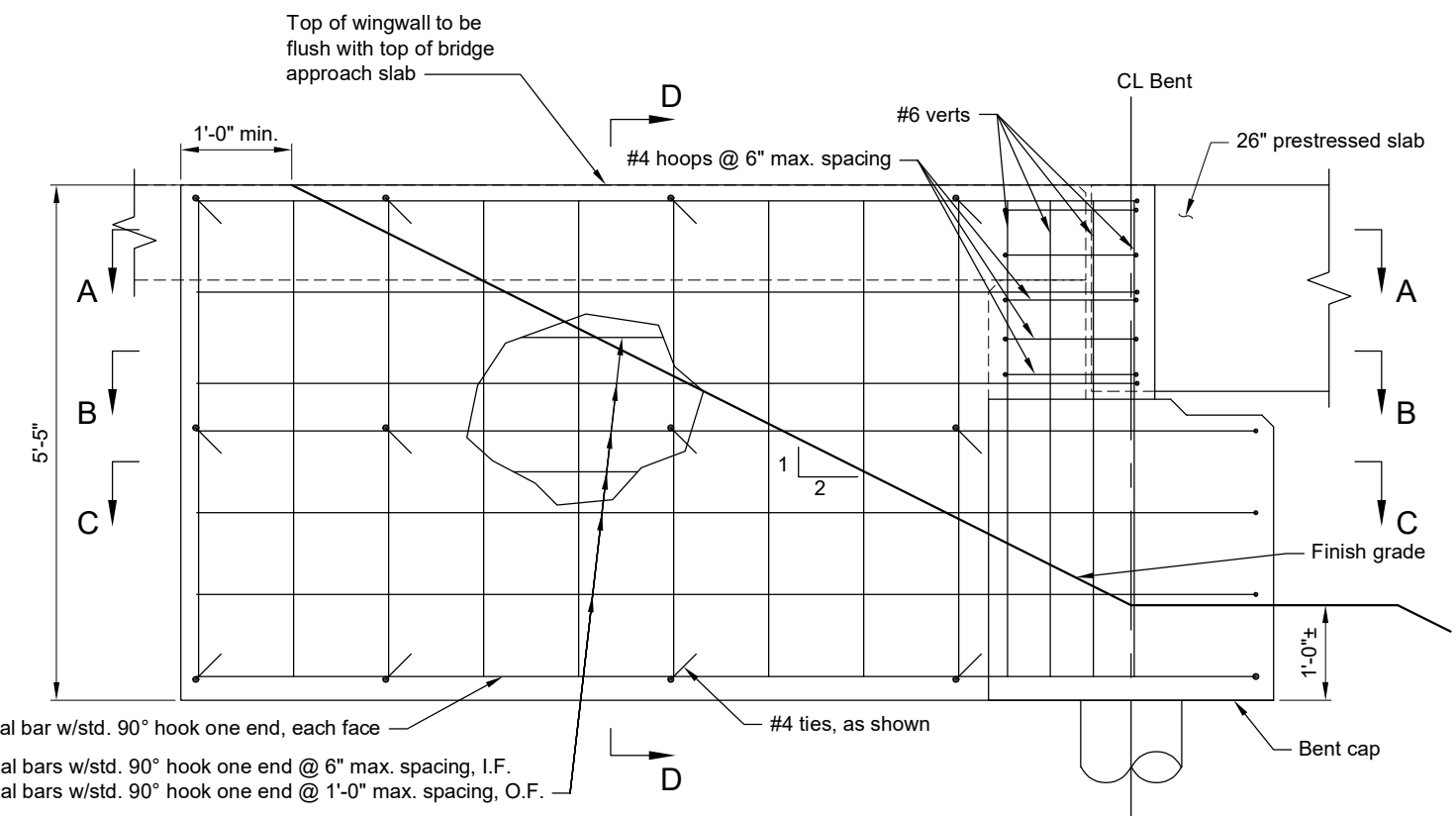


SECTION B-B
1/2" = 1'-0"

Note:
See Section A-A for
details not shown

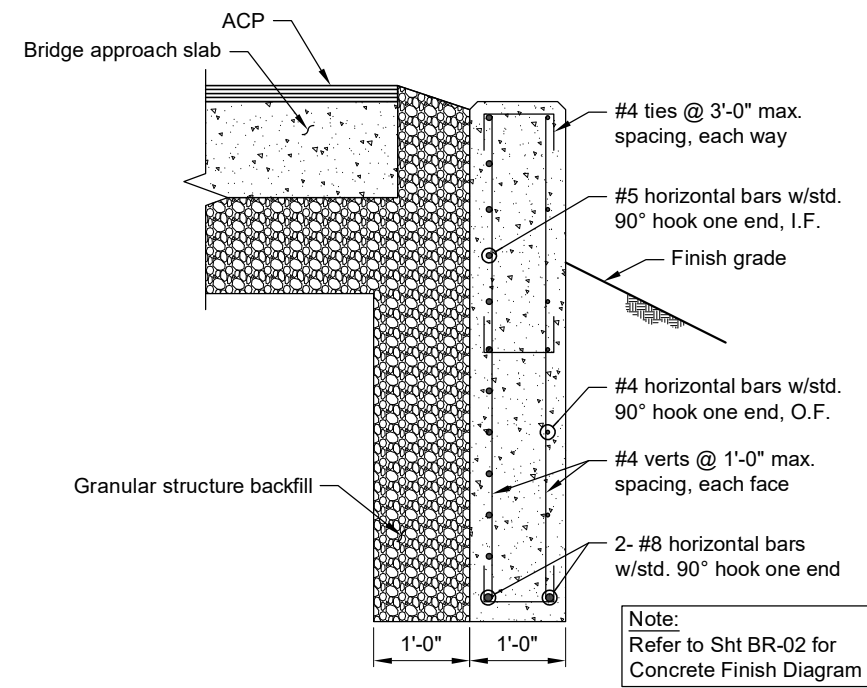


SECTION C-C
1/2" = 1'-0"



ELEVATION - BENT 1 (BENT 2 SIMILAR)
1/2" = 1'-0"

#8 horizontal bar w/std. 90° hook one end, each face
#5 horizontal bars w/std. 90° hook one end @ 6" max. spacing, I.F.
#4 horizontal bars w/std. 90° hook one end @ 1'-0" max. spacing, O.F.



SECTION D-D
1/2" = 1'-0"

Note:
Refer to Sht BR-02 for
Concrete Finish Diagram

WARNING
If this bar does not
measure 1" then drawing
is not to scale



LINN COUNTY ROAD DEPARTMENT
3010 FERRY STREET SW
ALBANY, OREGON 97322
PHONE: (541) 967-3919
FAX: (541) 924-0202
E-MAIL: Roads@co.linn.or.us

COUNTY COMMISSION
ROGER NYQUIST
CHAIRMAN
WILLIAM TUCKER
SHERRIE SPRENGER

ROADMASTER
WAYNE E. MINK, P.E.
COUNTY ENGINEER
DAINEAL MALONE, P.E.

DATE:	REVISION:	BY:

BRIDGE NO:	DATE:
0002-0306	1/13/2025
PROJECT NO: CB2102	
TRS: T. 12 S., R. 04 W., SECTION 17, W.M.	
DESIGNED BY: K. Groom	CHECKED BY: A. Potts
DRAFTED BY: K. Groom	REVIEWED BY: D. Malone

OWL CREEK - PEORIA ROAD
BRIDGE REPLACEMENT

LINN COUNTY
2025

WINGWALL DETAILS

SCALE: as shown
SHEET BR-09

REGISTERED PROFESSIONAL ENGINEER
17206PE
Digitally signed by Kevin M. Groom
Date: 2025.01.13 11:30:47-08'00'
OREGON
JULY 19, 1994
KEVIN M. GROOM
Expires: 6/30/25

K:\Projects - Current\BR 0002-0306 Owl Creek (Peoria Road) Bridge\KMG\CAD\0002-0306 Design.dwg